

North Main Street, Providence, RI Design Guidelines

University of Rhode Island
Department of Community Planning & Landscape Architecture
Community Planning Studio

Fall 2005



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Prepared for:

The Providence Planning Department

Prepared by:

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Acknowledgments

The Fall 2005 Graduate Community Planning Studio at the University of Rhode Island Department of Community Planning and Landscape Architecture was commissioned by the Providence Planning Department to prepare design guidelines for four commercial corridors in Providence: Broad Street, Chalkstone Avenue, North Main Street and Wickenden Street.

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Table of Contents

I– Introduction	p: 1-5
II– Existing conditions	p: 2-1 to 2-16
III– Vision	p: 3-1 to 3-18
IV– Design Guidelines	p: 4-1 to 4-40
V– References	p: 5-1 to 5-4
VI– Appendices	p: 6-1 to 6-31
A– Glossary	
B– Notes from the Workshop with providence Planning Department, Nov 7th 2005	
C– Notes from the Summit Neighborhood Association meeting, Oct 20th 2005	
D– Final presentation to Providence Planning Department	

I– Introduction

The Fall 2005 Studio, acting as a consultant to the City of Providence, Department of Planning and Development, was given the task to develop design guidelines for four distinct commercial corridors: Broad Street, Chalkstone Avenue, North Main Street and Wickenden Street. The streets are emblematic of the different types of commercial corridors in the city. The goal of these guidelines is to encourage high-quality building and site design while still allowing flexibility for designers. Different kinds of commercial corridors require their own set of design strategies to provide a model for future development and to become a tool for continued growth.

The Studio divided the work into three phases. Phase I provided a comprehensive inventory of current conditions. Phase II developed possible themes and visions and identified potential nodes to concentrate future commercial development. Phase III highlighted each team's goals for their respective study area and recommended appropriate design guidelines to accomplish the themes previously identified.

When constructing urban design guidelines for North Main Street, our first principle was to respect the existing context of the street through the following goals:

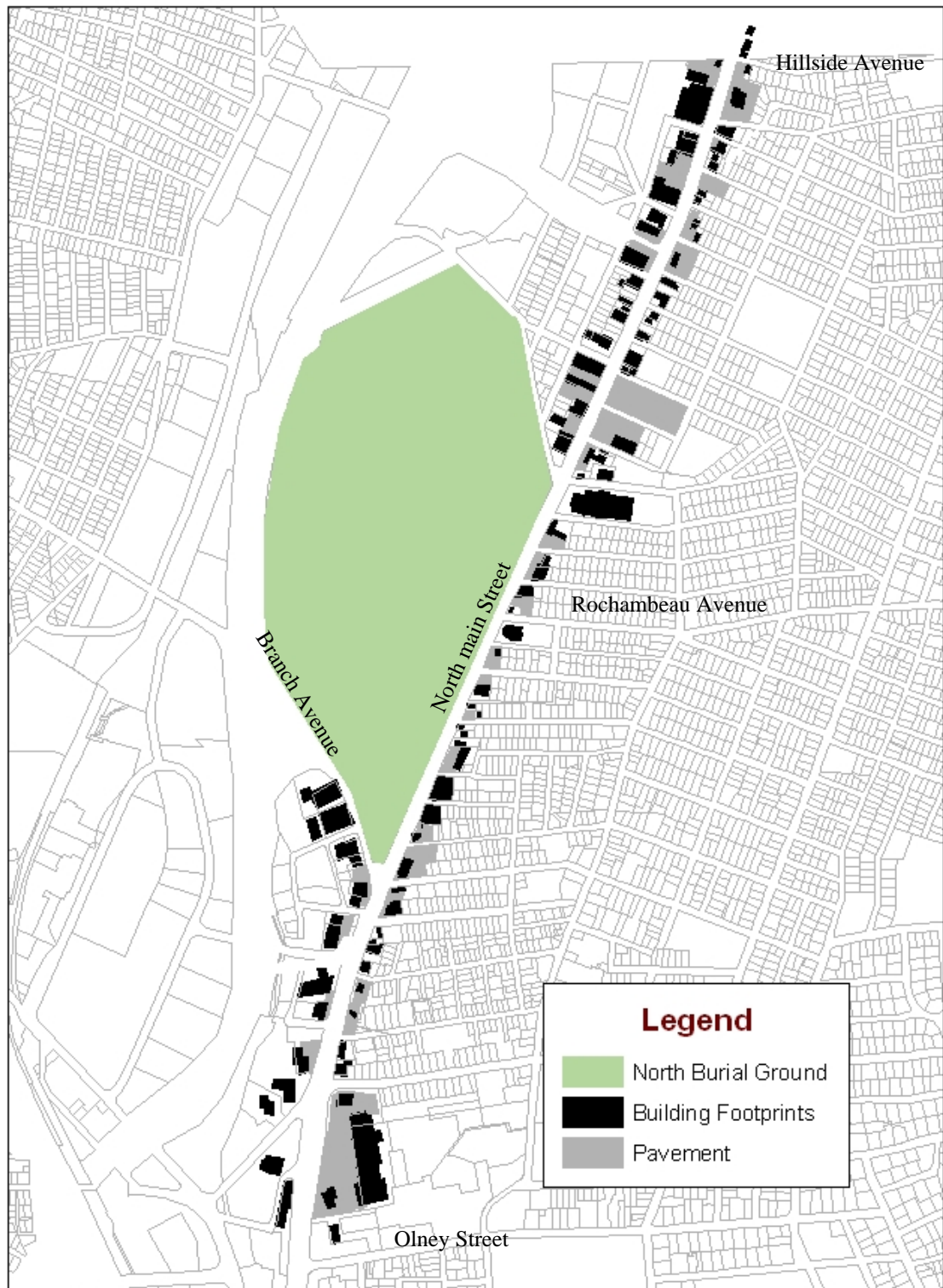
- Improving the visual character of the commercial corridor;
- Strengthening pedestrian access through landscape and building elements that will contribute to an attractive and physically safe environment for all consumers;
- Developing site amenities that contribute to an active and economically viable community;
- Promoting the development of a design that connect the street to its adjacent neighborhoods creates a relationship among a variety of commercial uses.
- Encouraging the use of high quality building materials;

A sense of place ensures a healthy community by strengthening the connections residents and consumers make to their environments. Through the utilization of the urban design guidelines, North Main Street has an opportunity to establish a sense of identity for all users, create greater pedestrian access and safety, and enhance connections to the surrounding areas.

II- Existing Conditions

- Map: the study area and figure ground
- Map: current land use
- Neighborhoods demographic profile
- Land use and proposed zoning
- Building characteristics and conditions
- Street level
- Circulation
- Parking and Lost spaces
- Landscaping
- Signage
- Trash and street furniture
- The key issues

Study area/ Figure ground



Source: RIGIS,
RI State Plane Feet 83,
City of Providence

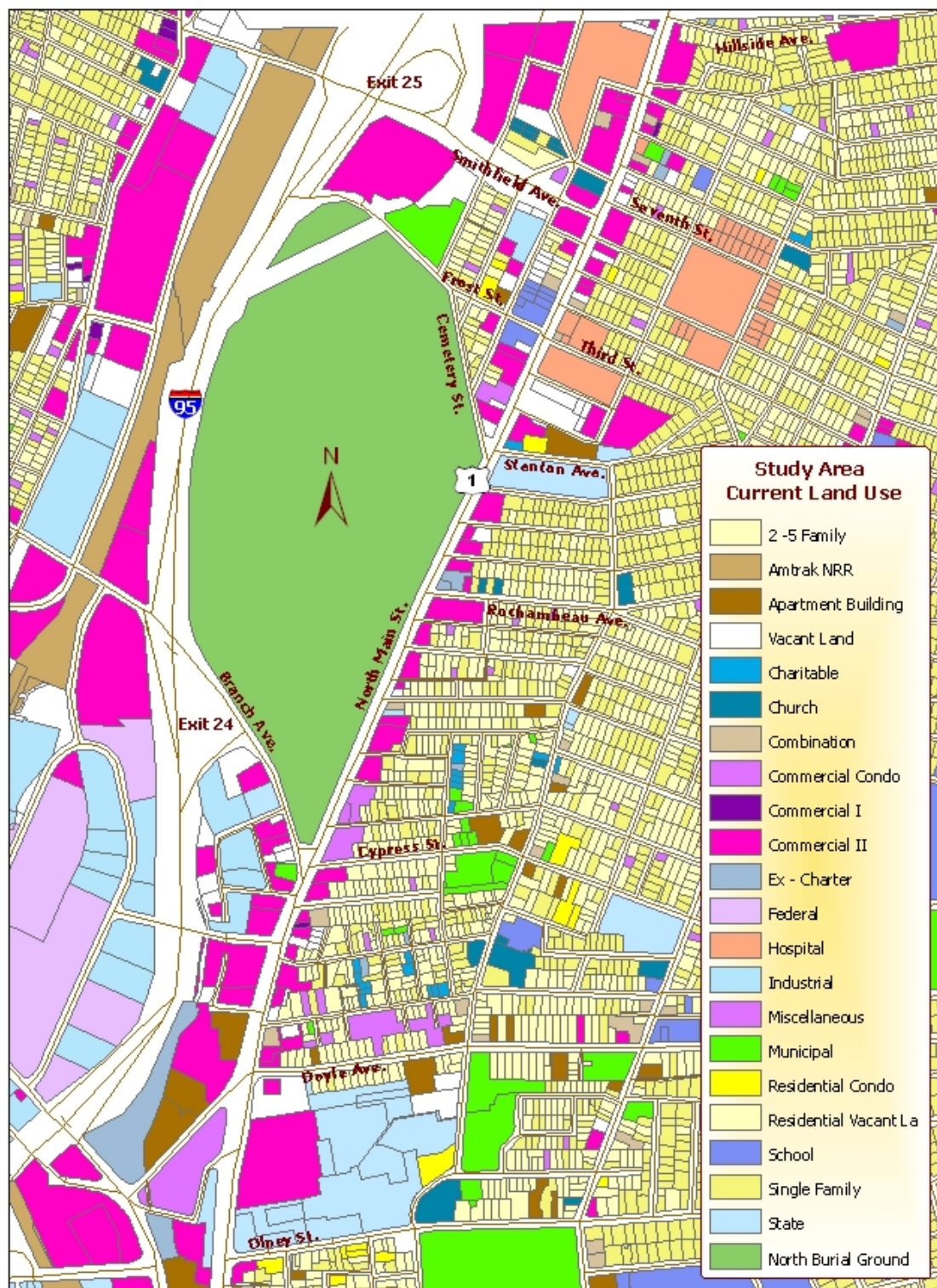


0 250 500 1,000
Feet



Cartography:
Wayne Barnes
December 8, 2005

Current land use



Source: RIGIS,
RI State Plane Feet 83,
City of Providence

0 500 1,000 2,000
Feet

Cartography:
Wayne Barnes
January 16, 2005

Neighborhoods demographic profile

Variables	Hope Average	Mount Hope Average	Citywide
Median HH Income	\$ 37,838	\$28,413	\$26,867
Families below poverty	4,9%	24%	23,9%
Racial composition	82,1%: White 8,2%: Black 4%: Hispanic	47,7%: White 29,4%: Black 10,5%: Hispanic	45,8%: White 14,5%: Black 8,8%: Hispanic
Crime (2002) per 1000 persons	2.3 shot fire calls 12.1 burglary 2.3 drug offenses 16.7 vehicle theft	14.3 shot fire calls 13.8 burglary 3.4 drug offenses 14.5 vehicle theft	7.1 shot fire calls 12.9 burglary 5.6 drug offenses 17.3 vehicle theft

North Main Street belongs to two neighborhoods quite contrasted.

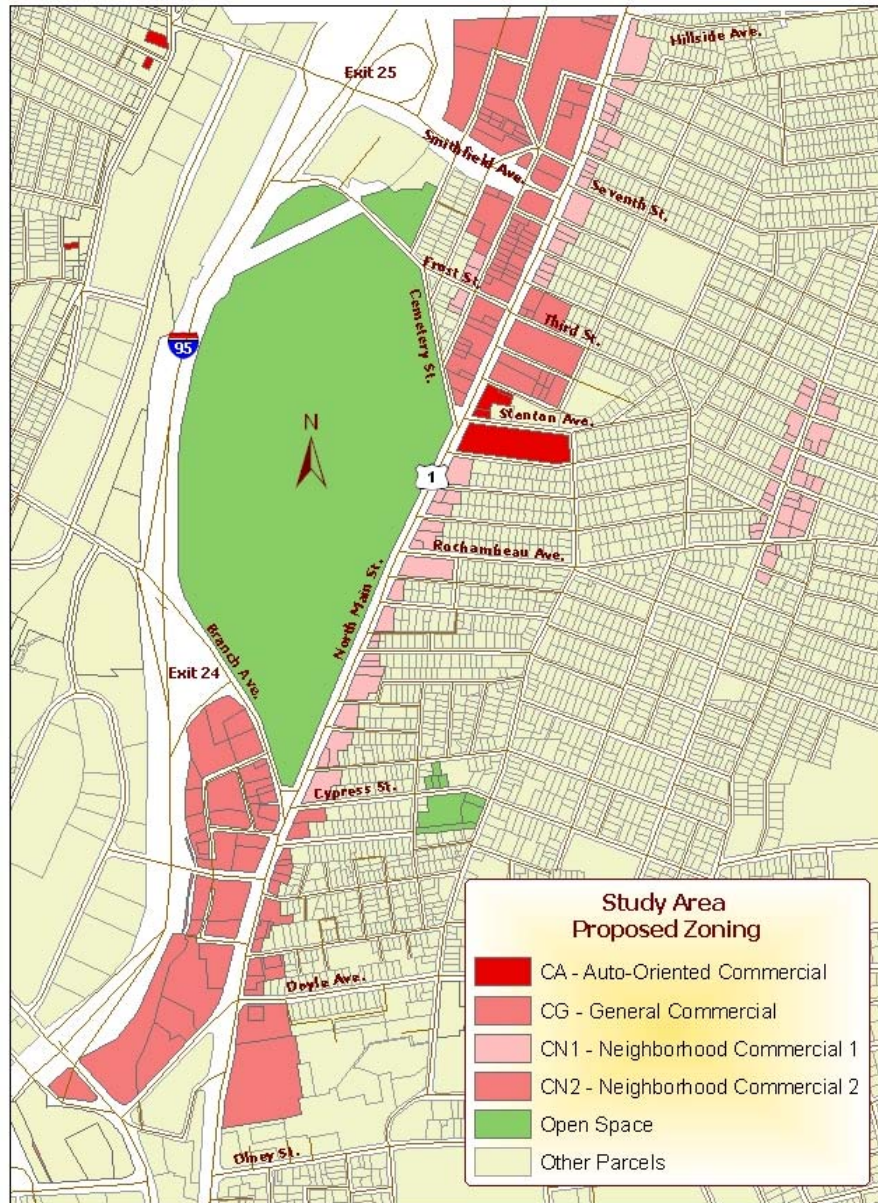
- Hope and Mount Hope differ notably racially and demographically. The population of the former is predominantly white (82.1%), and homeowners in the neighborhood are 85% white. People are well-educated, with 3 out of 4 persons age 25 or older completing requirements for their high school degree. This neighborhood enjoys a median household income of \$37,838, one-third higher than the citywide level and an unemployment rate of 6.6%, which is well below the citywide rate of 9.2%.
- Mount Hope is historically a more African-American neighborhood, and today this group accounts for almost 30% of the population. The neighborhood suffers from a quite high level of poverty; median household income is 20% below the citywide average. One in four households had income below the poverty level, and one in three children was poor.
- Housing values mirror these different demographics characteristics: Hope median housing value was in 1990 16% higher than the citywide median, and the median rent 38% higher. Mount Hope housing and rent values are close to the citywide figures.

Land Use and proposed Zoning

Actual land use

- 15% auto oriented commercial
- 50% other commercial (including retail and wholesale uses, restaurants, medical offices)
- 7% residential
- 18% vacant property or building space
- 5% private parking
- 5% other (religious, historical property, etc)

North Main Street Proposed Zoning



Source: RIGIS,
RI State Plane Feet 83,
City of Providence

0 500 1,000 2,000
Feet

Cartography:
Wayne Barnes
January 16, 2005

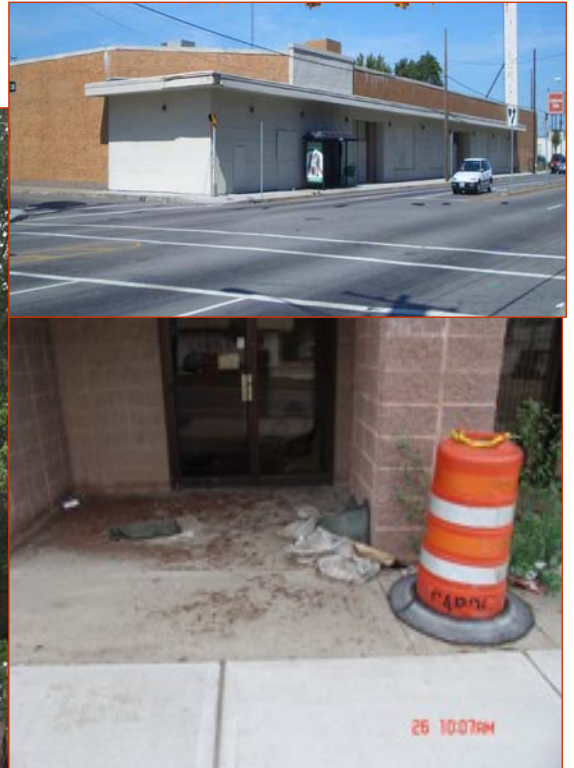
Building Characteristics

- Mismatched collection of styles, heights, building materials, colors, and roof lines.
 - Modern buildings of concrete and brick
 - Some older brick structures of 2 and 3 stories with gable roofs
 - Some historic buildings: armory, farm house
- Building height vary from 1 story to 3 and even 4. But most of the buildings are one story structures
- No alignment of architectural features such as windows and cornices



Building Condition

- Many of the original facades have been recovered, without using the original material
- Many of existing structures are vacant, in poor condition, or not maintained
- At least half are well maintained
- Variety of fences and walls scattered on the street: some are aesthetics (brick wall, steal cast iron fence), but a lot are unattractive such as rusted chain fences and barbed wire.



Street Level Facade

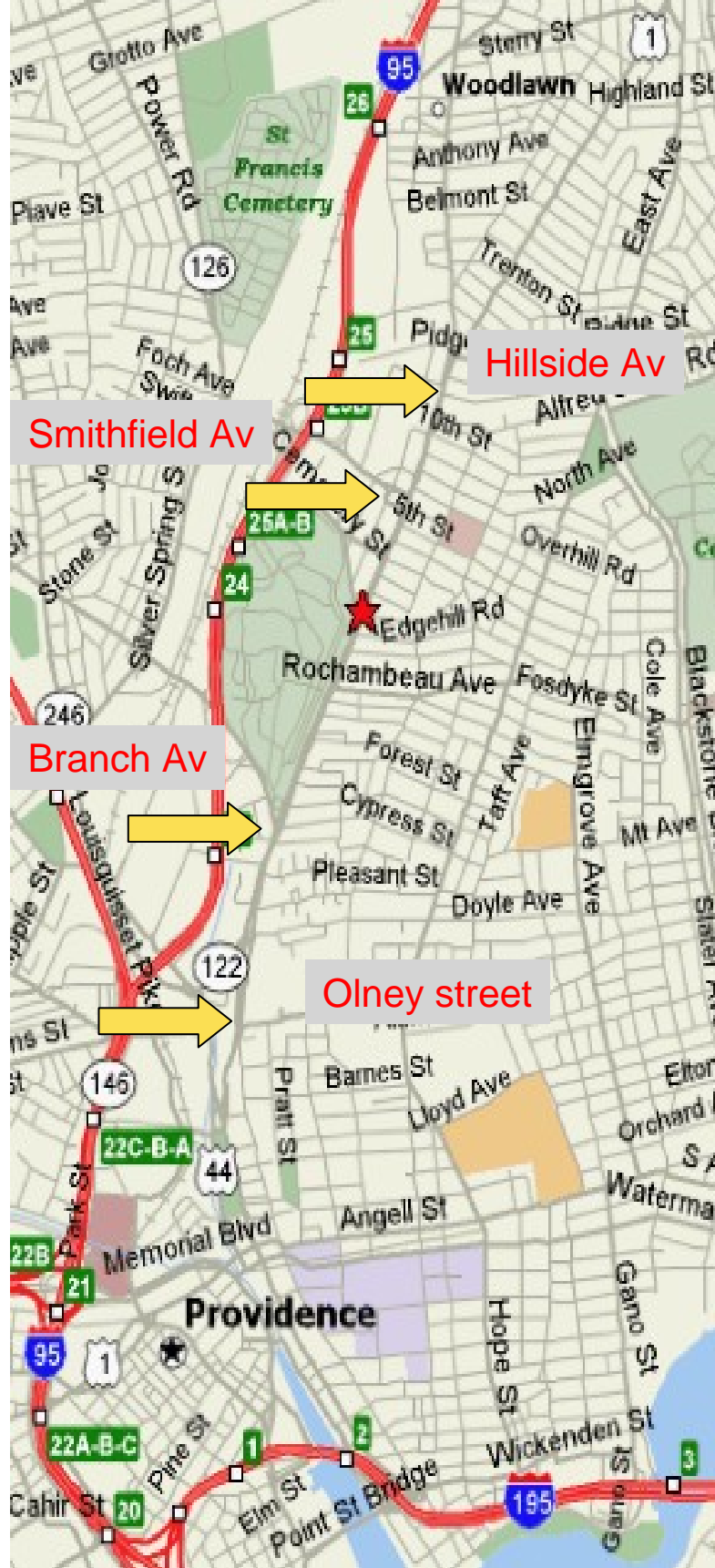
- No street wall created: setbacks are up to 50-100 feet
- Windows displays are non-existent and provide little interest to pedestrians
- Facades at street level are often renovated and do not match original building materials



Circulation: a transit corridor

North Main Street mainly accommodates the auto circulation:

- A transit corridor from Pawtucket to Downtown Providence
- A key access to I95 at Branch Ave. and at Smithfield Ave.
- Two travel lanes and one parking lane both northbound and southbound
- The posted speed limit is 25 mph, but averages between 30-40 mph
- The average daily traffic is 16000



Circulation: lack of pedestrian safety

- Crosswalks are scarce:
 - Only four crossing points at main or hazardous intersections:
 - Doyle, Cypress, Rochambeau and 3rd.
- An especially dangerous intersection at Doyle:
 - Several streets crossing
 - Few crossing points
 - A vulnerable population living in Charles Gate
- Countless curb cuts:
 - Auto related businesses (extremely wide curb cuts).
 - Fast food businesses (drive through curb cuts)



Circulation: a street poorly connected

- Many side streets give access to Summit and Mt. Hope Neighborhoods and to Hope street. But the side street intersections are poorly maintained and unattractive.



Parking: excessive on site

- Too many vacant parking spaces
- Large parking lots in front of businesses



Parking: some good practices

- Effective use of the slope of the land by putting parking on top of the business
- Shared parking lots between several business



- Parking on the side or back of business

Lost Spaces

North Main Street Figure Ground

- Many underused areas
- Unnecessarily large impervious surfaces: 45% of the frontage is constituted with off-street pavement
- More private parking than is needed



Landscaping: an attempt on $\frac{3}{4}$ of the street



Extensive median and sidewalk landscaping improve the appearance of otherwise undistinguished commercial properties...

...in contrast to more barren streetscape evident near the northern end of the street.



Signage: anything is possible

- Indicative of the haphazard manner in which the street has been developed over the decades



- In some areas, signs are conformed to a certain size and themes, that make them more attractive



Trash and street furniture

- There are only four trash cans over the 1.3 miles of the street, and two of them are located in the area of Whole Food Market
- Trash collects in shrubs, bus shelters...
- Some dumpsters are right on the street, without screening



- Seating : no benches on the entire stretch of the street. The only possibility to seat is under the bus shelter. But there are only 4 bus shelters on the street, three on the southbound and one on the northbound.
- Lighting: the lights are dedicated to cars and not to pedestrian circulation.

The key issues

- A transit corridor disconnected from the neighborhoods
- Lack of pedestrian safety
- Poor street level attractiveness
- Ineffective use of space
- Environmental issue linked to the auto-oriented areas:
 - Urban runoff loaded with pollutants
 - Large impervious surfaces resulting in a poor stormwater management
- Heterogeneous land use inconsistent with future zoning

III- The Vision



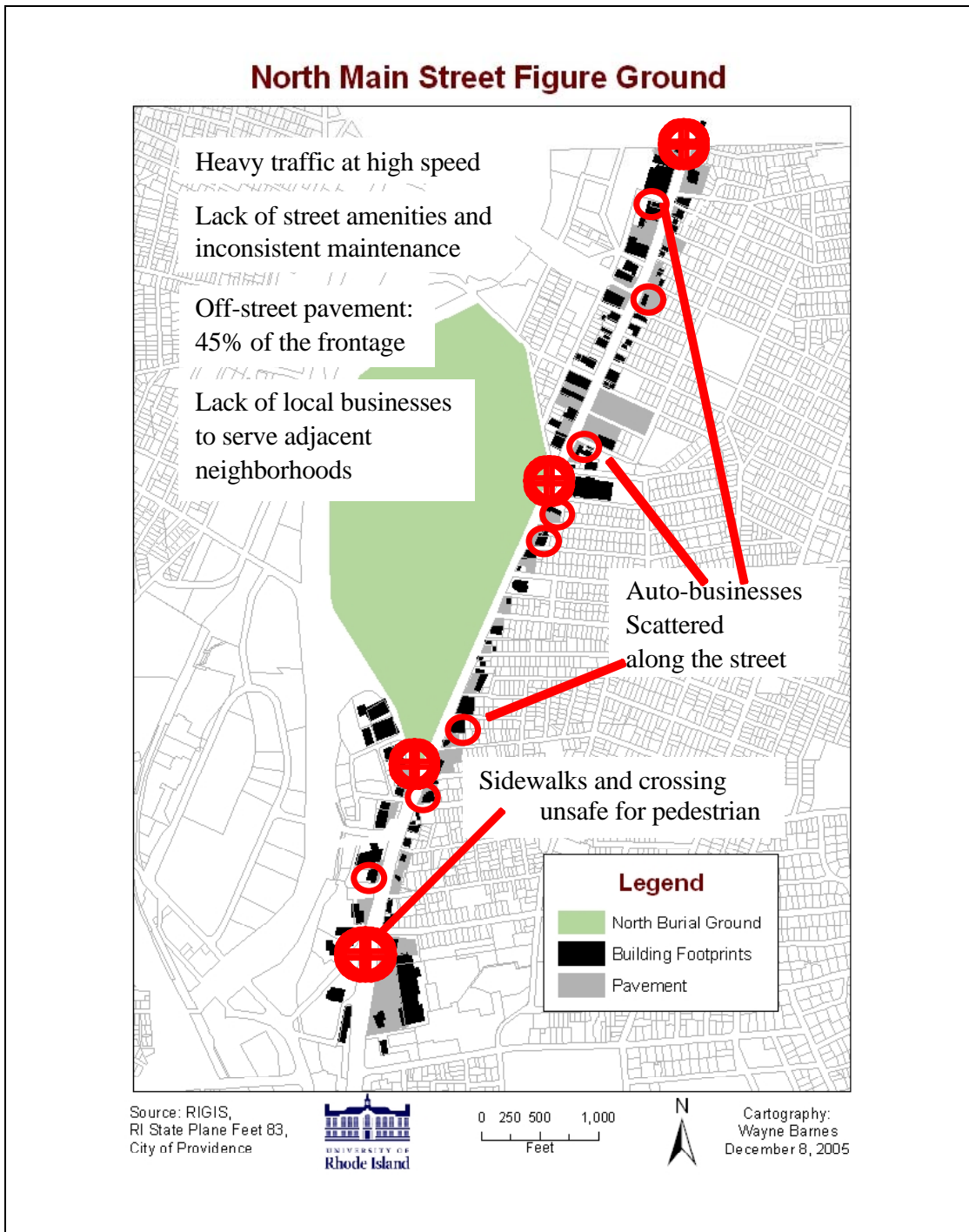
1- Street opportunities and constraints

North Mains street as a transit corridor faces some heavy constraints:

The characteristics of the street are shaped by its auto-corridor function. The whole street is organized around auto-businesses and auto-oriented businesses:

- Poor street level attractiveness
- Numerous auto-businesses spread all along the street
- Large curb cuts and drive through scattered all along the street
- Heavy traffic at a high speed
- Dangerous intersections for pedestrians as at Doyle
- Unaesthetic buildings for retail
- A lot of impervious surfaces mainly generated by on site business parking, and resulting in a very poor stormwater management: 45% of the surfaces is covered with off-street pavement
- Numerous adjacent streets that give access to North main (30 side streets from the east and 10 side streets from the west)
- Lack of local business
- Poor street amenities and inconsistent maintenance

See the map 1: Constraints on North Main Street



Map 1: constraints on North Main Street

On the other side, North Main Street benefits from some solid opportunities:

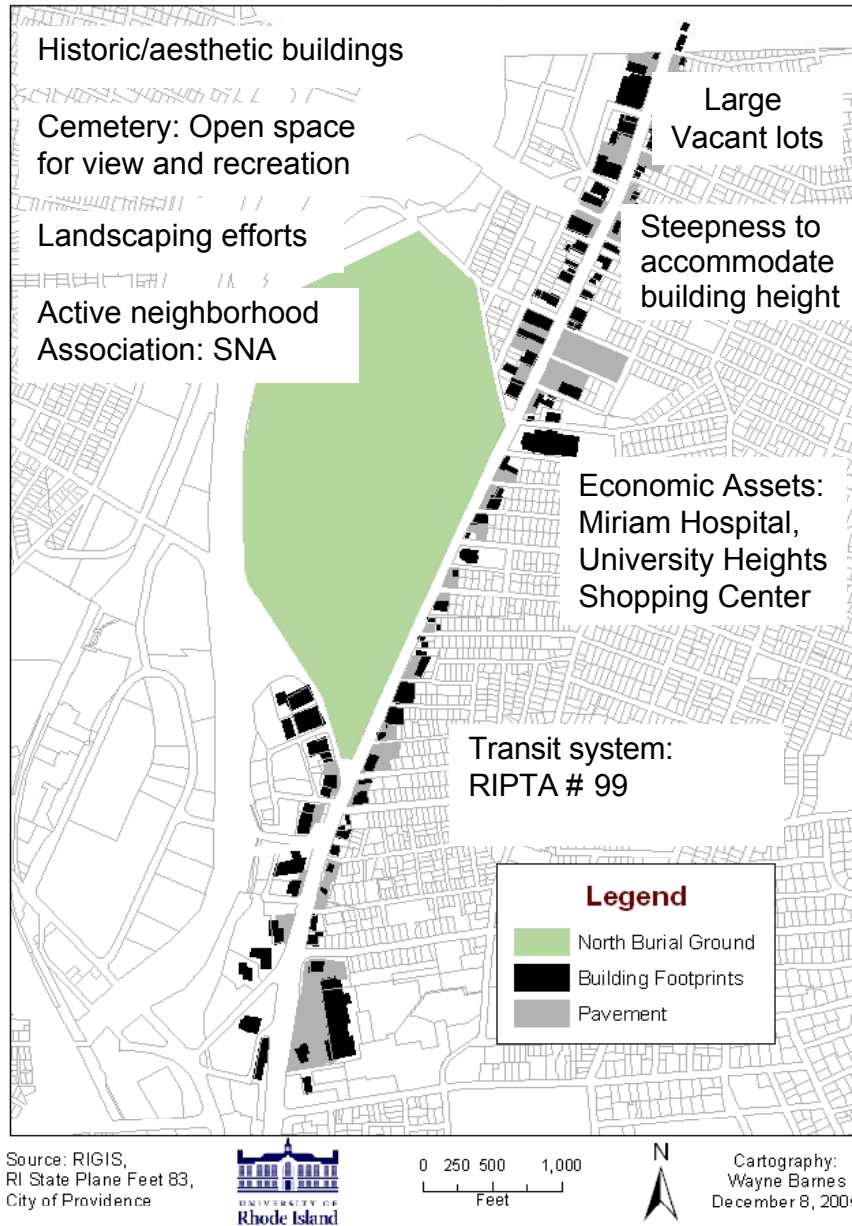
- Some historic or good-looking buildings: the historic farmhouse, the artillery, and some mixed use two to three story houses such as the house where is Miko.
- Large vacant lots that represent very good opportunities for future development, such as the former Sears building
- The Cemetery which is a good potential recreative area
- An active neighborhood association, the Summit neighborhood association
- The presence of some significant amenities such as Myriam Hospital, and the University Heights mall.
- An existing transit system to go downtown with the 99 RIPTA bus
- The steepness of the street that can accommodate some height

The access to the I95 is both an opportunity and a constraint:

- it generates the heavy vehicle traffic, and makes the street unsafe for pedestrian and polluted
- but it also brings to the street a large amount of potential customers for local businesses

See the map 2: Opportunities on North main Street

North Main Street Figure Ground



Map 2: Opportunities on North main Street

2- The Vision for North Main street

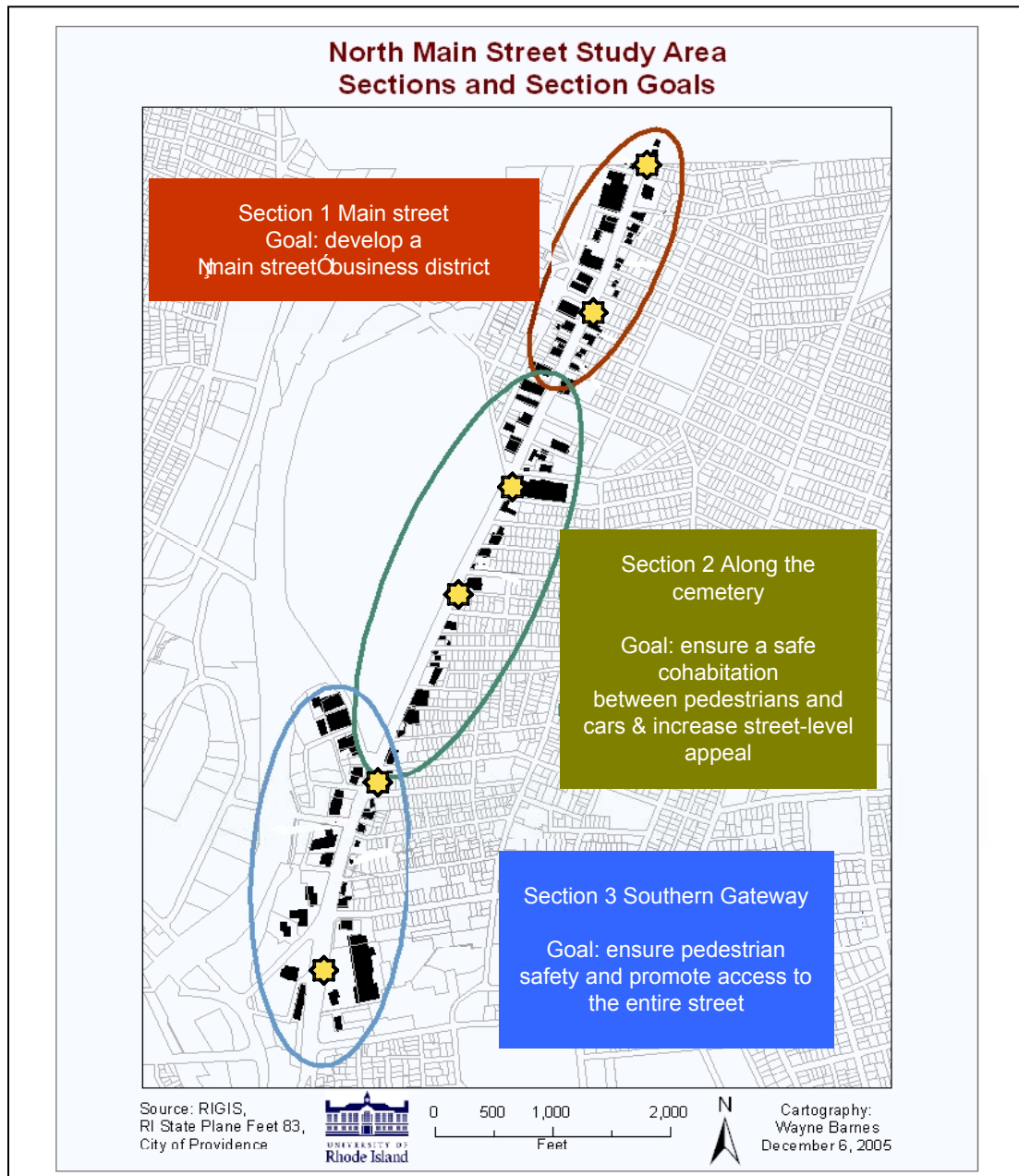
The values that shaped this Vision

The vision described below has been shaped by the following beliefs and goals regarding the future of the city and of the area of North Main Street.

- Predominance of gasoline powered automobiles will be coming to an end:
 - The use of cars within the city will be limited because of the congestion, the environmental concerns, the increasing of population density, and the increasing oil prices
 - The car will have to evolve towards more environmental-friendly technologies, for fuel and maintenance
- Nonetheless, cars will remain a major factor for North Main Street, at least in the future decade, and we must find ways to accommodate them along with pedestrians. They represent employment opportunities (especially for the Mount Hope neighborhood where unemployment is high). The two access points to I-95 are both a constraint and an opportunity because of traffic safety on one hand and potential customers on the other hand;
- North Main Street and the adjacent neighborhoods have an excellent potential to accommodate a higher residential density. These areas must be livable in order to attract more residents and more businesses;
- Community spirit is vital to the area, and the connection between North Main Street and the surrounding neighborhoods must be developed;
- The development of North Main street must complement the commercial areas on Hope Street (Rochambaud);
- The aesthetic appeal of North Main street must be improved, and take advantage of the street assets, such as the historic farmhouse, or the Artillery;
-
- Historic preservation and sense of community must be considered in future development.
-
- The management of stormwater must be improved

Three areas designed with three levels of goals

We have considered three sections mainly based on their function in the street and on their spatial characteristics. For each section we have designed a goal and suggested modifications for the future. Here after are also described the plazas that will connect the different sections, and the modifications we envision for the entire stretch of the street.



Map 3: the three sections and their goals

a. The Potential Main Street

See map 4

- Location: from Frost Street to Pawtucket town line
- Goal: develop a Main Street, pedestrian-friendly atmosphere
- Suggested modifications:
 - Develop street level attractiveness:
 - developing mixed retail
 - build on a connection to Pawtucket by use of brick building materials
 - creating transparent and attractive windows at the pedestrian level that match the architectural style
 - creating an appealing street wall by forbidding set backs, and generalizing brick-building materials,
 - landscaping sidewalks with green strips and trees,
 - developing a green park in place of the vacant lot under the basketball court
 - requiring screening of dumpsters, auto-businesses, and parkings
 - Ensure pedestrian safety through traffic calming (use of raised crosswalks) and landscaping on the sidewalk...



*Livermore,
CA, draft
design
guidelines,
p-220*

b. Along the Cemetery

See map 5

- Location: From Branch Avenue to Frost Street
- Goals: Ensure the cohabitation of cars and pedestrians, and increase the street attractiveness for pedestrians
- Suggested modifications:
 - Ensure pedestrian safety by:
 - forbidding drive through businesses if they are not auto service related,
 - minimizing curb cuts,
 - reducing vehicle speed with traffic calming (speed bumps, one-way side street, and on-street parking),
 - adding landscaping along the sidewalk.
 - Create vibrancy through:
 - encouraging mixed-use buildings (see good building example on North Main Street),
 - favoring mom and pop local businesses suitable to the neighborhood (grocery, bakery, coffee shop, general store, movie theatre...),
 - creating a night life on the street by developing the suitable businesses: family restaurants, movie theater, ...
 - discouraging formula businesses
 - Direct the area toward a more pedestrian-friendly future:
 - creating a zoning CA (for auto-businesses) at the access roads to the I95, meaning on Branch Ave and Frost Ave.
 - checking the compliance of existing businesses with environmental regulations
 - screening auto businesses and parking
 - Reduce the space dedicated to parking:
 - encouraging on street parking,
 - requiring shared parking among neighboring businesses,
 - locating on-site parking at side or rear of building,
 - developing a long range goal of underground parking
 - Take advantage of the cemetery as a recreation space, and provide an access (crosswalk, gate) in front of Rochambaud street.



A compromise to diminish the visual impact of this business and allow the customer to see the cars. Providence picture by W.Barnes modified by Joe Maia.



A screening to reduce the visual impact of this parking lot. Picture taken by W.Barnes and modified by Joe Maia.

c. Southern Gateway Area

See appendix map 6

- Location: from Olney Street to Branch Avenue
- Goal: Ensure pedestrian safety and promote access to the entire street.
- Suggested modifications:
 - Ensure pedestrian safety for street crossing at the Charlesgate location:

- more crosswalks on each side of the intersection
- a pedestrian-only cycle for the street light
- a footbridge between the Charlesgate building area and the Whole food parking, with a ramp to access on the sidewalk
- Promote the image of the entire street by use of wayfinding signs



Providence, RI, picture by F.Flourens on Dec 2005, with pedestrian bridge from Dunant-Steg, Stuttgart, Germany, 8 June 2000 by Nicolas Janberg

d. Plazas

Plazas will serve to connect the three areas as well as the adjacent neighborhoods.

- Locations: At the intersections of North Main Street and Branch, Rochambeau, Cemetery, Smithfield, and at the entrance from Pawtucket
- Construction:
 - See example
 - Amenities to include bus shelters, benches, trash cans, lighting, landscaping, and signs to promote the neighborhoods.
 - Amenities, signs, and landscaping are designed with complimentary styles, materials, and colors. Color may correspond to the neighborhood identification on street signs.



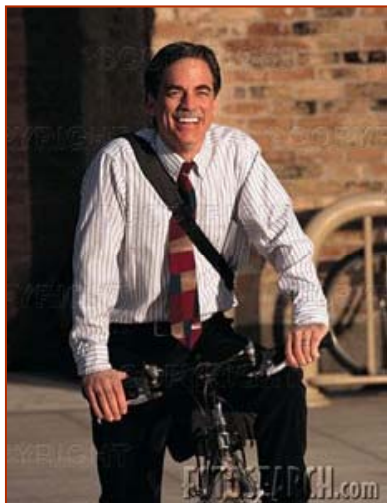
Livermore, CA, draft design guidelines, p-220

e. For the Entire Street

See map 7

- Building massing and style:
 - traditional buildings: no setbacks, mixed use with residence on the second and third floor, brick building materials
- Signage:
 - placement on awnings, facades, or on vertical signs on the setback
 - vertical signs will have a height limitation of TBD
 - no signs on the roof nor temporary signs
 - use of indirect lighting
 - material:: TBD
- Landscaping:
 - sidewalk with green strips to break down the impervious surfaces, trees (make sure that the maintenance is taken care of by the business owner, neighborhood assc., or by the State)
 - screening for parking, auto-businesses, dumpsters, and utility hardware
 - green roofs wherever the building makes it possible
- Parking:

- shared parking on the existing lots: the parking cannot be reserved for customers
- on-street parking
- parking behind the building and for new development-underground
- A multi-modal street
 - increase mass transit service along the street to encourage people to explore the whole length of the street
 - add a bike path on both side of the street
 - plan the building of a street car that would connect Pawtucket to Cranston through North Main street and South Main street



3- For implementing this Vision

The guidelines proposed in Phase III are the first step to implement this vision. In addition, we have identified the following recommendations that would be necessary in order to “re-create” North Main Street.

a. Public investments:

In order to build the pedestrian footbridge, the bike path and the street-car, the public investment will be necessary. This can only derive from a State decision to invest heavily into a transit-oriented future state city.

b. Regulations

The creation of the CA zoning along Branch Av and Smithfield Av requires a Providence Planning Department adaptation of the new zoning, such as it has been designed so far.

See the map 2 and 4 to locate the proposed CA zones.

The enforcement of environmental laws would be key in ensuring that auto-businesses do not add pollution to the street. This would require an action in cooperation with DEM.

In addition a modification of the building code could be helpful in order to acknowledge the necessity for buildings to be compliant with the environmental laws. This code could also help preventing the pavement of unutilized spaces on private properties. Such space should be at least left with grass and mowed.

c. Financial incentives

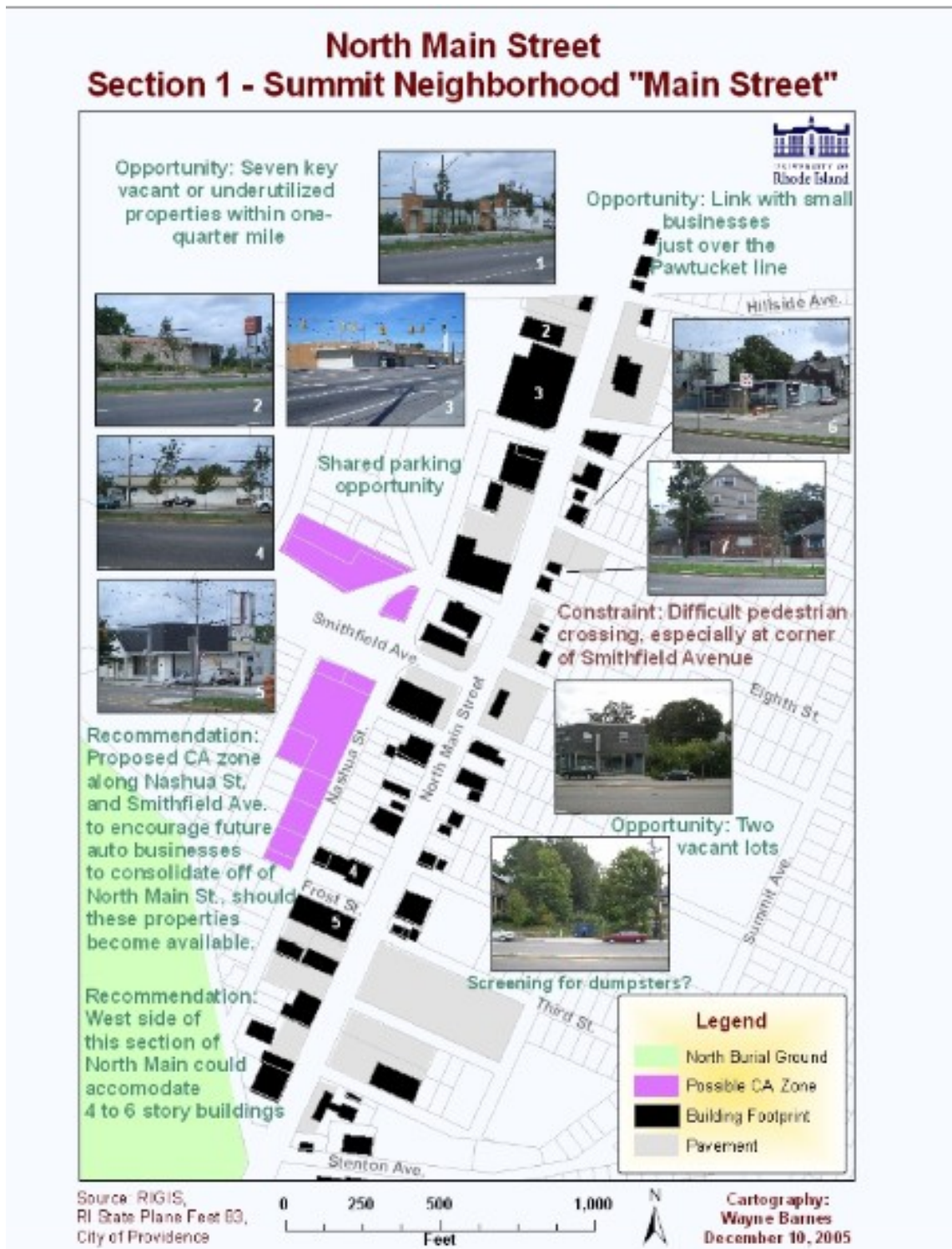
To incite developers to build according to the design guidelines some incentives can be developed such as the following:

- A new development that does not fit with the CN1 and CN2 limitations will have to trade the following features; minimum impervious surface, green roofs, building of one of the plazas, retention/treatment amenities for parking run off...

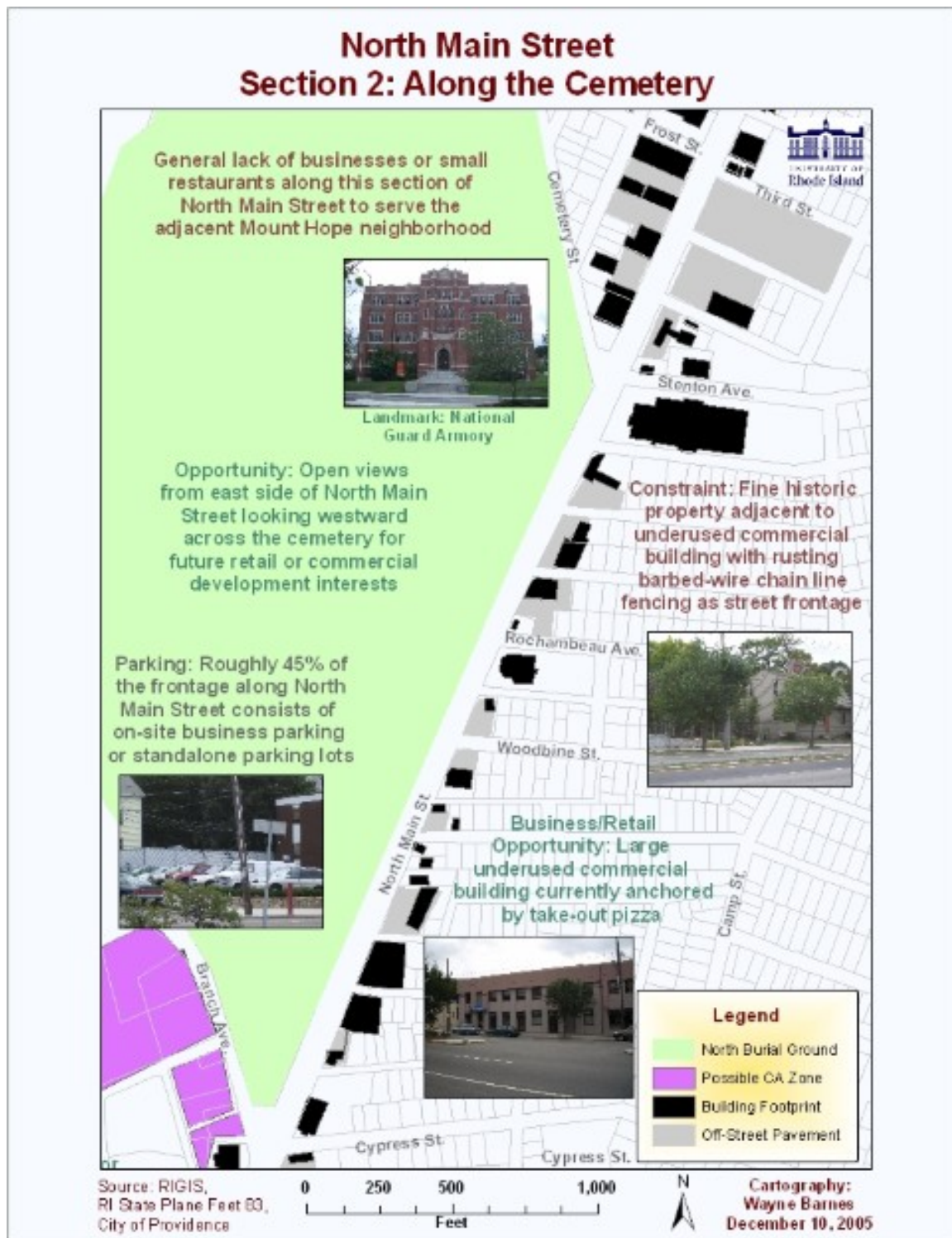
d. Grants and financial support for local communities

The State project for street market offers grants for landscaping improvements to businesses or neighborhood associations. These grants can then be shared between business owners to landscape their portion of street. An association like the Summit Neighborhood Association could compete for this grant and then be in charge of dispatching the funds and coordinating the improvements projects.

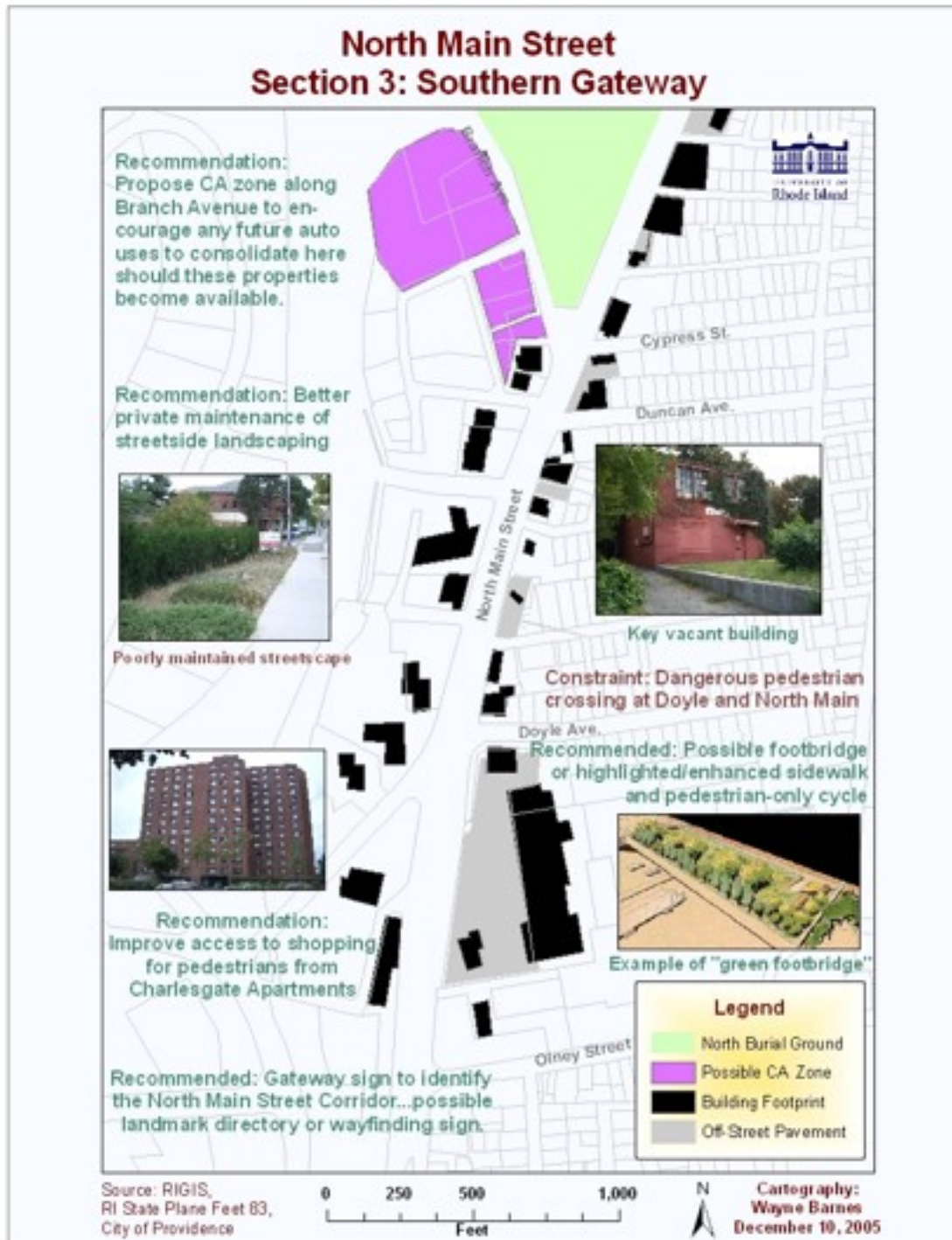
Map 4: section 1 Main street



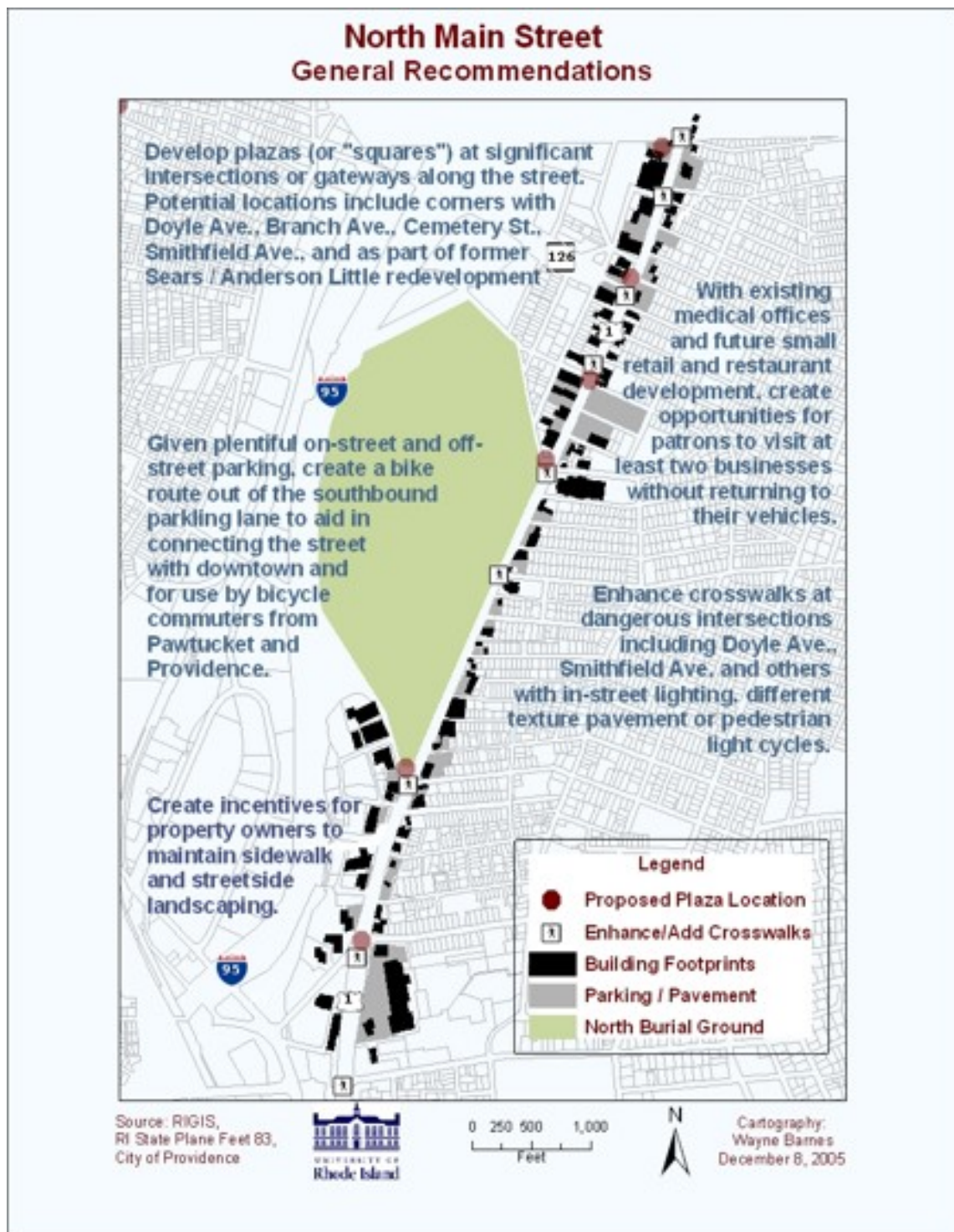
Map 5: Section 2 Along the cemetery



Map 6: Section 3 Southern gateway



Map 7: Recommendations for the entire stretch of the street



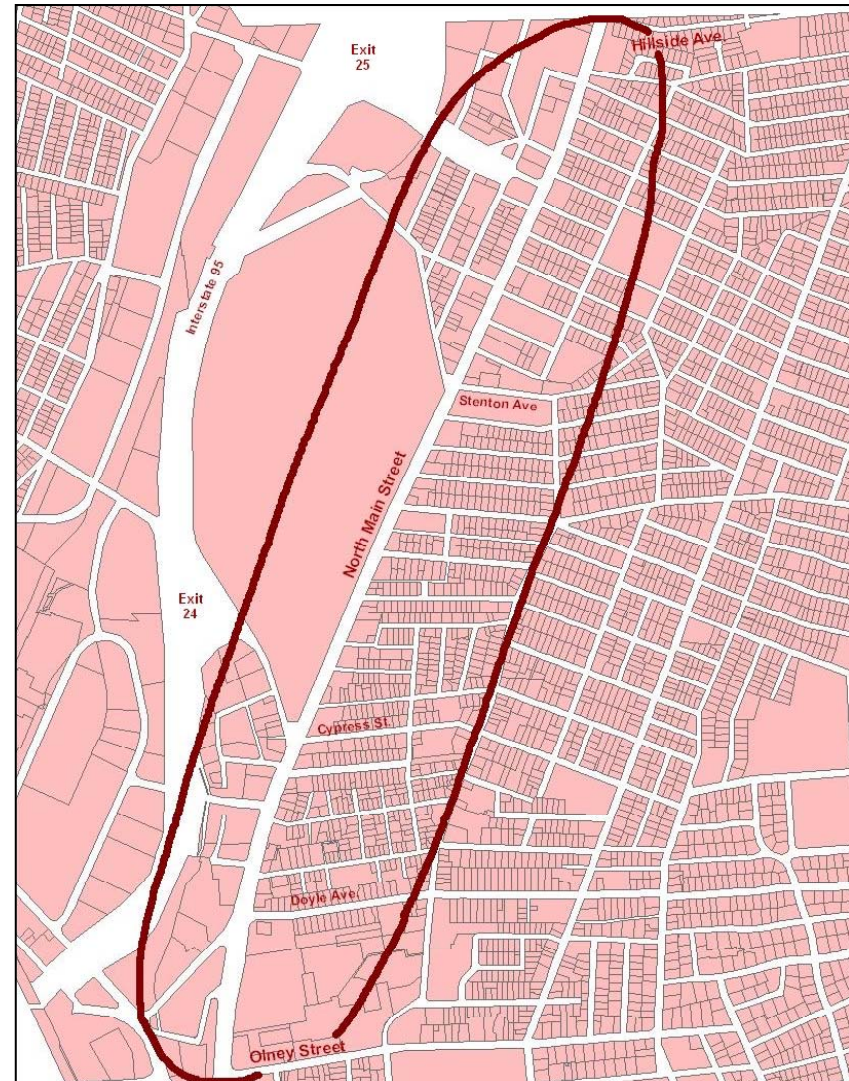
IV- Design Guidelines

Purpose of the guidelines

This document intends to provide to developers and to the Providence Planning Department guidelines for the redesign of the North Main Street area in Providence. The design guidelines are based on the vision exposed in the section III. Its main purpose is to help redesign the street to increase the pedestrian safety, and the attractiveness of the street level facades. It is mainly directed at the commercial corridor, and does not include any residential guidelines, except for recommendations concerning mixed-use buildings. This document is for recommendation only.

Target area

These guidelines apply to the 1.4 miles of North Main Street extending from Olney Street in at the southern end to Hillside Avenue and the Pawtucket Line on the north.



City of Providence North Main Street

Building Siting and Orientation

Intent: The intent of the building design guidelines is to create a “Main Street” atmosphere with a street wall of mixed use development built to the edge of the sidewalk. Entrances onto the street and window displays offer a safe and interesting pedestrian friendly environment.

Figure 1.1
Corner Site

Zoning Requirements:

- 1.1-a A new building must extend along a minimum of 60% of the lot frontage.
- 1.1-b A minimum and maximum setback of zero feet is required.
- 1.1-c New buildings on corner lots must be built to the corner of the front and side lot lines, with a minimum and maximum setback of zero feet from the front and side streets.
- 1.1-d A waiver for a setback of 1-7 feet may be sought from the Commission to allow for architectural treatments, a public plaza, outdoor seating or an additional landscaped area.
- 1.1-e On corner lots, requirements for Front Lot Line Treatment and First Floor Treatment above shall apply to the side lot lines also. An entrance is permitted on the corner of the building.



Figure 1.2
Setback of zero feet



Building Siting and Orientation

Recommendations

- 1.1 Buildings should have limited alcoves and recesses along sidewalk edge. (Figure 1.3)
- 1.2 Buildings should be oriented to North Main Street.
- 1.3 Entrances to buildings located at the intersection of North Main St. and side streets should be oriented to the corner. (Figure 1.4)
- 1.4 Public space at the corner should be scaled in a manner that allows for pedestrian flow and encourages social interaction.
- 1.5 Front of building to cover 100% of lot frontage is encouraged when access between buildings is not necessary.
- 1.6 At least 70% of businesses should serve mainly pedestrian walk-ins.



Figure 1.3
Buildings create a street wall



Figure 1.4

Building Form and Design

Height

Zoning Requirements:

2.1-a Minimum height: 2 stories. 1 story building may be permitted by waiver if surrounding buildings are also one story. Maximum height: 4 stories.

Recommendations

- 2.1-1 Buildings should be predominantly two or three stories.
- 2.1-2 Four to six story buildings are allowed upon completion of view shed studies done from any residences behind building in question. Placement of these taller buildings is strongly encouraged on the west side of North Main Street.

City of Providence North Main Street



Figure 2.1

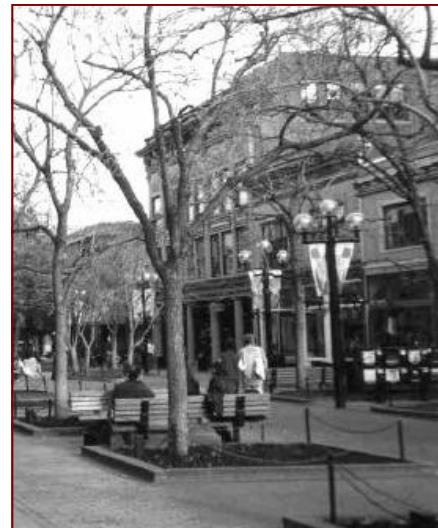


Figure 2.2

Scale and Proportion

Zoning Requirements:

- 2.2-a The first floor shall have a minimum building height of 14 ft. to the primary cornice line.
- 2.2-b Architectural detailing shall delineate building floors. (figure 2.4).

Recommendations

- 2.2-1 Buildings should be designed with the human scale in mind. (Figure 2.3).
- 2.2-2 Architectural features will distinguish between upper and lower floors. (figure 2.5).
- 2.2-2 Large expanses of building wall should be architecturally divided into bays of 30ft. by using details such as columns, windows, and changes in texture.

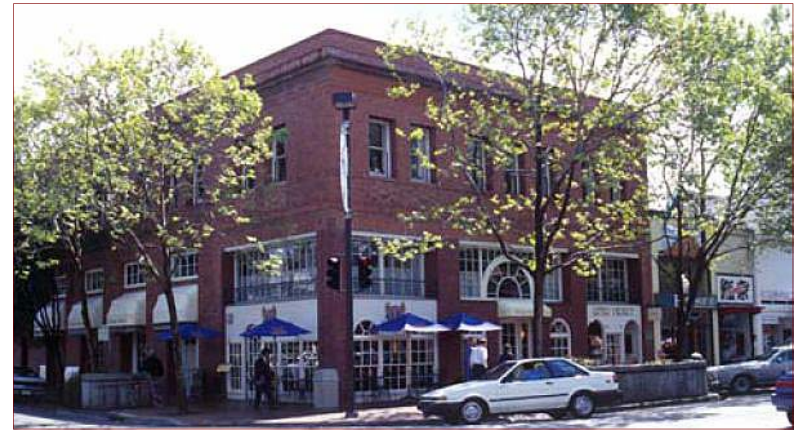


Figure 2.3

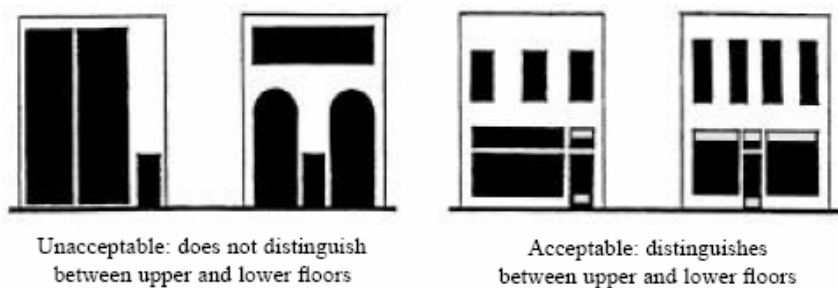


Figure 2.5

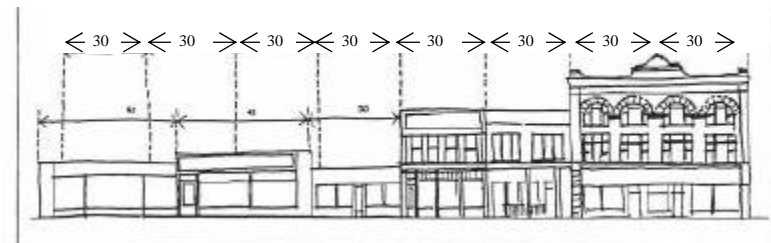


Figure 2.4

Facades and Entrances

Zoning Requirements:

- 2.3-a Building entrances shall be along street front. On lots with two or more sides of street frontage, the primary entrance shall face the primary street.
- 2.3-b Entrances shall be directly accessible from the public sidewalk.
- 2.3-c There shall be an entrance every 35 ft. of building frontage.
- 2.3-d Solid surface roll down window guards are prohibited. Alternatives to solid roll down grates include ornamental wrought iron doors, interior roll-down grates or other security devices that can be completely concealed during regular business hours.

Recommendations

- 2.3-1 First floor should be well-defined, employing a richer use of materials, architectural details, textures, and awnings to create a visual interest.
- 2.3-2 Creation of a rhythm with architectural elements such as windows, doors, and cornices is strongly encouraged.
- 2.3-3 Articulation of entrances using a diversity of materials, textures, columns, and architectural detailing is strongly encouraged.



Figure 2.6



Figure 2.7

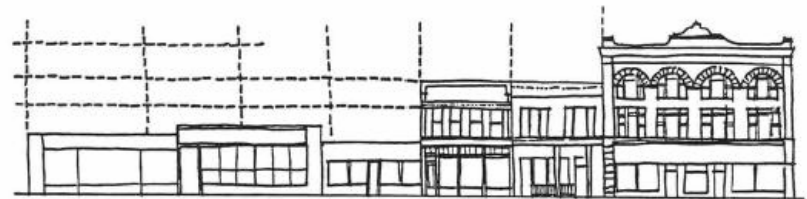


Illustration of Architectural Feature Alignment on the Block Face

Figure 2.8 . Architectural alignment.

Building Form and Design

Roofline

Recommendations

- 2.3-1 Top level of buildings should be defined with cornices or other architectural features.
- 2.3-2 Articulation of roofline should be designed to be consistent with the neighboring businesses and the historic architectural style of the street.



Figure 2.9



Figure 2.10



Figure 2.11



Figure 2.12

City of Providence North Main Street

Materials, Colors, and Textures

Zoning Requirements:

- 2.4-a The primary building materials on the façade shall be brick, limestone, sandstone, granite, terra cotta, cast stone, glass, metal, or other similar material.
- 2.4-b Materials shall be compatible with the existing streetscape.
- 2.4-c The use of non-durable materials such as exterior insulated finishing systems (EIFS) and other similar products shall be prohibited on the first floor and discouraged on upper floors.

Recommendations

- 2.4-1 Materials, colors, and textures should be consistent with those that predominate on the street (i.e. red brick).
- 2.4-2 Color will be used to highlight architectural features.
- 2.4-3 Color of buildings should be compatible with their surrounding
- 2.4-4 Similar architectural theme should be employed along the corridor.



Figure 2.13

Window Patterns and Articulation

Zoning Requirements:

- 2.5-a On all street frontages, the first floors shall have windows covering a minimum of 70% of the building façade between the height of 2-12ft. From the ground, with transparent glass.
- 2.5-b There shall be a maximum distance of two feet between windows or transparent surfaces.
- 2.5-c The building façade on upper floors shall have a minimum of 25% and a maximum of 70% transparency with transparent glass.

Recommendations

- 2.5-1 Windows should be kept clean for greatest possible visibility
- 2.5-2 Windows on different floors should be consistent in height, width and detail to create reasonable uniformity along the street.
- 2.5-3 Windows which display merchandise in an attractive, uncluttered manner are highly recommended.
- 2.5-4 Window displays should be lit during the evening hours.



Figure 2.14



Figure 2.15



Figure 2.16

Awnings and Canopies

Recommendations

- 2.6-1 Awnings and canopies should not be more than one storefront in length.
- 2.6-2 Awnings constructed of fabric with a matte finish are strongly encouraged.
- 2.6-3 Plastic awnings and illuminated awnings are strongly discouraged.
- 2.6-4. Lowest part of an awning or canopy should be at least 8 feet above the sidewalk.



Figure 2.17

Number of Spaces and Parking Standards

Intent: The views of large expansive paved surfaces dominate the North Main Street corridor. Both north and southbound travel provides a parking lane in addition to numerous on-site parking facilities. Much of the on-site parking has frontage on North Main Street without any buffer or screening. In general, there is an abundance of empty parking spaces on a consistent basis. Future development should make every attempt to minimize parking facilities.

Zoning Requirements for Number of Spaces:

In general, two (2) spaces are required per 1,000 square feet of gross floor area (GFA).

3.1-a CN-1 & CN-2: Parking shall not exceed the number of spaces required for each use listed in *Table 3.5*

3.1.b CG & CA: Parking shall not exceed 2 times the number of parking spaces required for each use listed in *Table 3.5*

See *Table 3.5 Regulations for Non-Residential Uses in Commercial Zoning Districts* for specifics.

Recommendations:

3.1-1 Whenever possible, developments should utilize the available on-street parking before proposing on-site facilities. (Figure 3.1)

Zoning Requirements for Parking Standards:

3.2.a See *Section 2.8.4* for required minimum size of parking space, minimum size of driving aisle, and additional requirements.

Recommendations:

3.2-1 Parking lots should be designed to minimize their environmental and visual impacts.

3.2-2 Development should minimize the amount of impervious surface.

3.2-3 Design should promote the use of compact automobile spaces.



Figure 3.1

Location and Layout

Zoning Requirements for Location and Layout:

- 3.3-a Surface Parking: Parking shall be located on the same lot as the principal structure or use the parking is intended to serve. See *Section 2.8.1*
 - 3.3-b Parking Garages: The design of the parking garage shall have the appearance of a horizontally storied building. The first floor of any parking garage shall be an occupied use.
- See *Table 3.7 Building, Site and Parking Design Regulation for Commercial Districts* for specifics.

Recommendations:

- 3.3-1 It is recommended that on-site parking be located to rear or side of the primary building. No more than 25% of the total parking should be located on the side of the structure. (Figure 3.2)
- 3.3-2 It is recommended that parking areas between two (2) structures serve both structures. (Figure 3.3)
- 3.3-3 It is recommended that where practicable, parking facilities utilize the grade of the land to accommodate parking above the structure.
- 3.3-4 The style of parking garages should correspond architecturally to the building it serves.
- 3.3-6 To minimize the scale, it is recommended that parking areas be broken into smaller landscaped lots. (Figure 3.4)
- 3.3-7 Layout of parking lots should be designed to promote pedestrian safety.

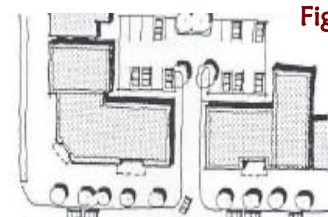


Figure 3.2

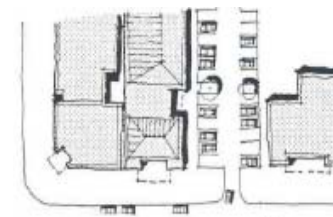


Figure 3.3

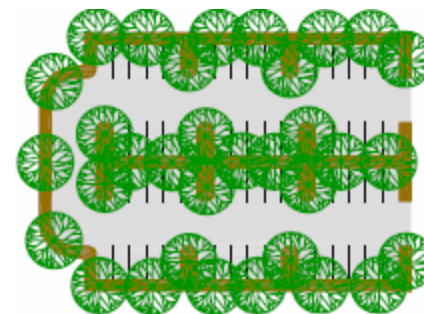


Figure 3.4

Landscaping

Recommendations:

- 3.4-4 In areas where parking lots abut North Main Street, a berm, a dense vegetative hedge, or hard buffer, or any combination of all three, should be provided. (Figure 3.5, 3.6)
- 3.4-5 Chain link fences should not be used to define the boundaries of a parking lot area.
- 3.4-6 Landscaping should be used to clearly define the parking areas from the road right-of-way as well as highlight parking lot entry and exit points. (Figures 3.6 and 3.7)
- 3.4-7 Entrances/exits of parking areas should allow for adequate visibility for vehicle operators.



Figure 3.5

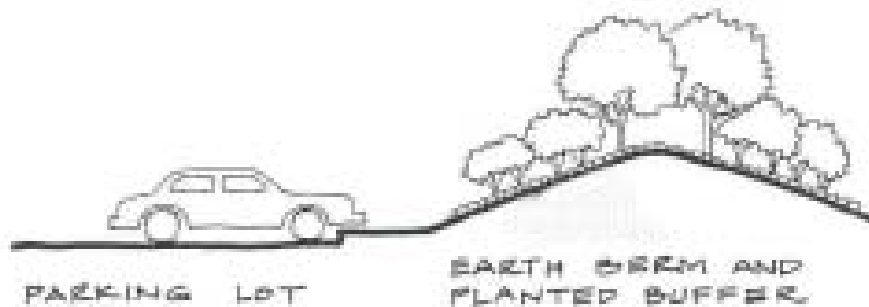


Figure 3.6



Figure 3.7

Landscaping

Zoning Requirements for Parking Area Landscaping:

- 3.4-a Landscaped Islands: A) Shall be a minimum of ten (10) feet wide and should be curbed with vertically faced concrete or granite curbing. B) Shall not be less than two hundred (200) square feet in area. C) Minimum of one (1) tree per island. See *Section 2.10.4*
- 3.4-b Frontage on North Main Street: Any portion of the front lot line that is not occupied by a building in a Commercial Zone must provide a minimum of a forty-two (42) inch high fence or brick wall. At least one shrub is required to be planted every ten (10) feet abutting the wall. Vehicular use areas for five or more vehicles shall be landscaped in accordance with *Table 2.4*

Recommendations:

- 3.4-1 Parking bays should be separated by a ten (10) foot landscaping strip, vertically faced with granite curbing. (Figures 3.8)
- 3.4-2 One planting is recommended per parking space along one length of the island.
- 3.4-3 Landscaped strips are recommended between areas where pedestrian walkways and parking spaces abut. (Figures 3.9)



Figure 3.8



Figure 3.9

Parking Lot Lighting

Zoning Requirements for Parking Lot Lighting:

3.5-a Parking: Any lighting used to illuminate parking areas shall only be directed downward and shall not spill into adjacent properties.

Recommendations:

- 3.5-1 Lighting design should be an integral part of overall site design
- 3.5-2 Fixtures should be arranged in such a way to provide equal illumination coverage across parking areas. (Figure 3.10)
- 3.5-3 The maximum height for parking lot poles is twenty (20) feet.
- 3.5-4 All pole-mounted lighting should shine a minimum of twenty (20) degrees below the fixture's horizontal plane. (Figure 3.11)
- 3.5-5 Pedestrian walkways that are incorporated into parking lots should have pedestrian level lighting at such an interval to provide sufficient coverage.
- 3.5-6 Parking lot lighting should not interfere with other lighting functions such as building lighting.

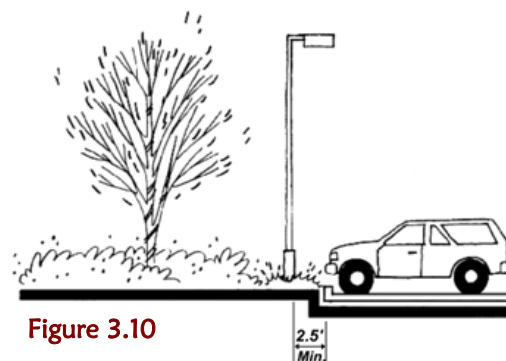


Figure 3.10

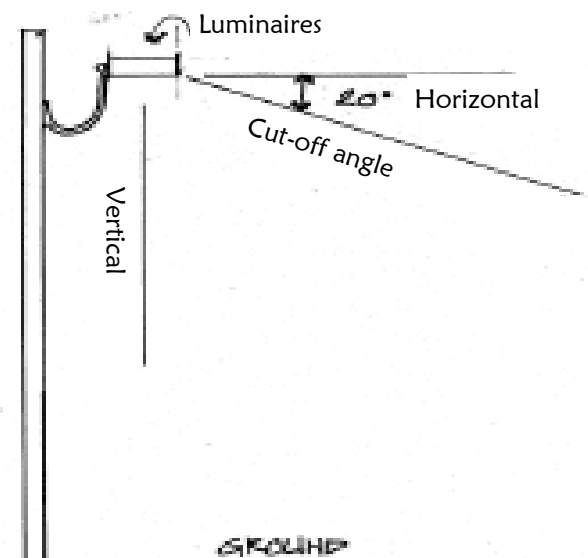


Figure 3.11 Twenty degree angle

Materials and Loading Areas

Zoning Requirements for Parking Material:

- 3.6-a All on-site parking areas must be improved with a compacted base not less than four (4) inches thick, surfaced with asphalted concrete, or a comparable all-weather dustless material.

Recommendations:

- 3.6-1 Efforts should be made to use porous pavement materials, as approved by the City Engineer.
- 3.6-2 Developers should provide adequate drainage for each parking area which should be presented in the form of a drainage plan. See *Landscaping Section* for additional recommendations

Zoning Requirements for Loading Areas:

- 3.7-a Number: *Educational and Health Care Facility Uses* shall provide one (1) space for 25,000 – 50,000 square feet of GFA. *Cultural, Entertainment & Recreation, General Service, Trade, and Industrial Uses* shall provide one (1) loading space for 10,000 –50,000 square feet of GFA.
- 3.7-b Size: Each loading space shall be 55 feet in length, 12 feet in width and allow for 14 feet of clearance.
See *Section 2.8.5*

Recommendations:

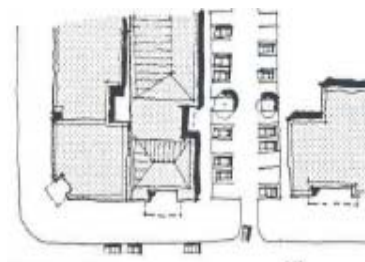
- 3.7-1 Whenever possible loading facilities shall be located to the rear of the structure. If loading spaces must be located on the side of the structure, then both a hard buffer (i.e. fencing) and a vegetative buffer is recommended along the side facing North Main Street.
- 3.7-2 Loading areas that have frontage along North Main Street are strongly discouraged.

Circulation

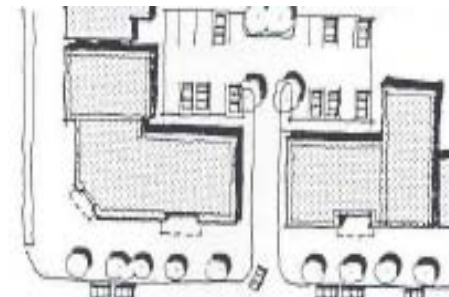
Intent: The current environment is very much geared towards automobile circulation and access. It is necessary to take into consideration the needs of pedestrians and bicyclists as well. Therefore it is important to improve circulation and access to benefit all users.

Zoning Requirements:

- 3.8-a Circulation: Pedestrian walkways, driveways and parking areas shall be designed with respect to topography, integration with surrounding streets and pedestrian ways, number of access points to streets, general interior circulation, adequate width of drives and separation of pedestrian and vehicular traffic so as to reduce hazards to all parties.
- 3.8-b Bikeways: North Main Street is in the process of becoming a signed bikeway. Automobile operators should be aware of the cyclist's right to share the roadways.



Figures 3.12 and 3.13
Limited use of curb cuts



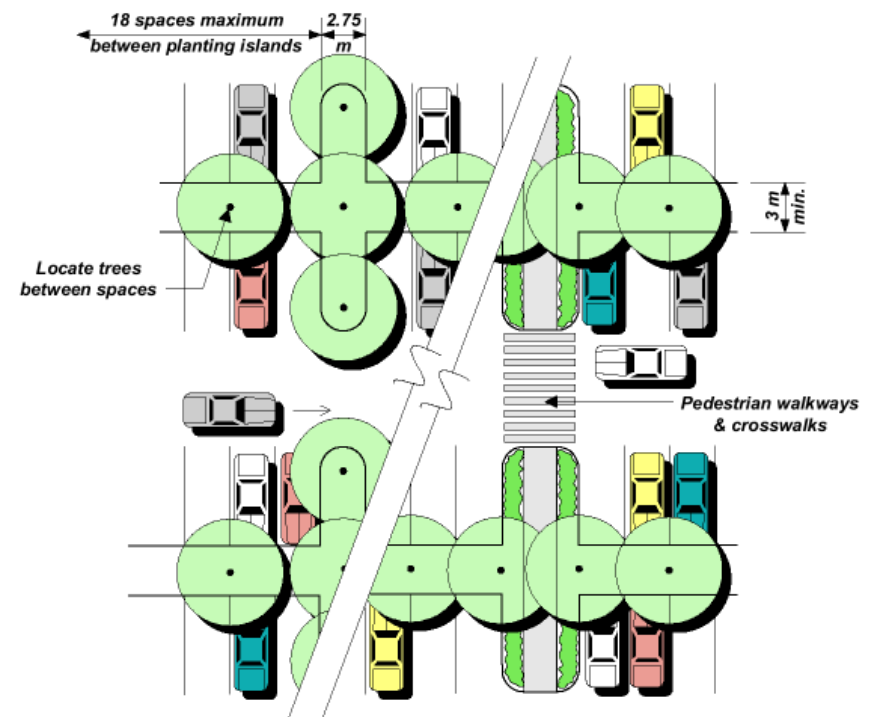
Recommendations:

- 3.8-1 Parking lots should be designed in such a way that the pattern of circulation is clearly identifiable by users.
- 3.8-2 The use of curb cuts along North Main Street should be minimized in number and width to provide for safer pedestrian flow.
- 3.8-3 Developments on corner lots should make use of the adjacent side street by locating parking lot entrances there, rather than on North Main Street. (Figure 3.12, 3.13)
- 3.8-4 If a curb cut is necessary, it should be no wider than fifteen (15) feet across and be at least thirty (30) feet away from another curb cut in either direction.
- 3.8-5 Pedestrian access should be provided by a developer in the form of a system of on-site walkways which provide the pedestrian with direct access to and from: *A)* primary building entrances, *B)* public sidewalks, *C)* parking facilities.

Circulation continued

Recommendations:

- 3.8-6 Recommended paving for pedestrian surfaces are: *A)* Stone, such as slate or granite, *B)* brick, *C)* concrete pavers.
- 3.8-7 In areas where pedestrian walkways cross over parking lot entrances, exits or lanes of travel, a crosswalk made of a distinctly different material than that of the parking surface is strongly encouraged. (Figure 3.14)



Figures 3.14 *Pedestrian Crosswalks*

General Landscaping

Intent:

Landscaping guidelines are intended to promote an urban environment that is in harmony with the surrounding natural environment. Environmental quality can be improved through the retention and installation of vegetation, reversing the trend of damage to the local environment. This can result in an improvement in the appearance of commercial, industrial, and residential areas, resulting in increased land values. Ultimately these measure can aid in protecting the health and welfare of Providence citizens and visitors.

Trees enhance the environmental and aesthetic quality of the City while reducing and softening the visual impacts of buildings, parking areas, public rights-of-way and adjoining properties. Trees, grass strips and hedges help break up the impact of large impervious services such as sidewalks and parking lots. Trees and landscaping enhance the value and marketability of an area. Additionally, trees provide oxygen, shade and ambiance.

Street Trees

Intent: The City recognizes the substantial economic, environmental and aesthetic importance of the trees and plantings throughout the community.

Zoning Requirements:

- 4.1-a The City Forester shall maintain a list of approved and prohibited plant species.
- 4.1-b The terms “small,” “medium,” and “large” refer to the size of a tree at the time it is installed or retained regardless of its species. *See table 2.1 of Zoning Ordinance .*
- 4.1-c Street trees are required for all new development.
- 4.1-d One canopy tree is required for every 50 feet of street frontage, with a minimum of one tree per lot.

Recommendations

- 4.1-1 One street tree every 30 linear feet is recommended to create a uniform canopy.
- 4.1-2 Street trees on the sidewalk should not block pedestrian access to storefronts or building entrances.
- 4.1-3 Placement of trees and landscaping should not obstruct vehicular access or circulation to driveways or parking areas.



Figure 4.1 *Effective use of street trees.*

Street Trees continued

Recommendations

- 4.1-5 Street trees shall not be planted within ten (10) feet of fire hydrants, utility poles, sanitary sewer, storm sewer or water lines, or within twenty (20) feet of streetlight standards or street intersections, or within five (5) feet on an existing street tree.
- 4.1-7 The City may require the property owner to remove, thin, prune, spray, stake, water and fertilize street trees adjacent to his or her property,
- 4.1-8 Street trees located in the public right-of-way directly adjacent to the property line of the lot may be counted toward the canopy coverage for the lot.
- 4.1-9 Trees that have been damaged or removed should be replaced with a similar species or a native species approved by the city forester.



Figure 4.2

Landscaping and Screening

Raised Divider Island

Intent: Trees, hedges and other plantings are important tools for creating a sense of place and a unifying theme that is appealing and welcoming. They serve as visual and physical barriers dividing lanes of traffic and acting as a traffic calming tool.

Recommendations:

- 4.2-1 A continuous divider island will be located between the northbound and southbound travel lanes running the length of North Main St. from Olney St. to the Pawtucket line. (Figure 4.4, 4.5)
- 4.2-2 The island will be a maximum of 10 feet wide to a minimum of 2 feet wide and shall be curbed with vertically-faced concrete or granite to prevent damage from automobiles. (Figure 4.3)
- 4.2-3 One street tree every 30 linear feet is recommended to create a uniform canopy and linear appearance.

From Doyle Ave. to Third St:

- 4.2-4 The placement of one hedge every 5 linear feet is recommended to prevent erosion.
- 4.2-5 Various plantings are encouraged between trees and hedges.
- 4.2-6 5 foot (width) breaks in the divided island will be provided to assist pedestrian crossings. Location of breaks: Third St., Stenton Ave, Rochambeau Ave, Cypress St, and Doyle Ave.

From Third St. to Hillside Ave:

- 4.2-7 The entire ground area of the divider island will be lined with red brick.
- 4.2-8 Tree pits are required (3' x 3').

City of Providence North Main Street



Figure 4.3

Figures 4.4 and 4.5 Before and after photos showing enhancement of North Main Street median with add vegetation.



Figure 4.4



Figure 4.5

Landscaping and Screening

Streetscape Landscaping

Intent: Grass strips help break up the large expanse of impervious surfaces such as sidewalks and parking lots.

Recommendations:

- 4.3-1 A continuous grass strip is recommended on both sides of the sidewalk. (Figures 4.6, 4.7, 4.8, 4.9)
- 4.3-2 The grass strip will be located 6 inches from the curb and 1 ½ feet in width.
- 4.3-3 It is recommended that the perimeter of the strip be lined with red bricks.
- 4.3-4 The placement of grass strips should not obstruct vehicular access, circulation to driveways, parking areas, access to storefronts or building entrances.

Figures 4.6 and 4.7 Before and after photos displaying effect of added landscaping and grass strip along sidewalk.

Figure 4.9 Demonstrates potential improvement to similar area along North Main Street (Figure 4.8)



Figure 4.6



Figure 4.7



Figure 4.8



Figure 4.9

Landscaping and Screening

Landscaping for Parking Lots

Intent: The purpose of requiring landscaping within parking lots is to minimize the visibility of large paved surfaces from neighboring streets and buildings. Landscaping acts a physical and visual barrier between cars and people.

Zoning Requirements:

4.4-a Vehicular use areas for five or more vehicles shall be landscaped in accordance with Providence Zoning Ordinance.

Table 2.4 Vehicular Use Area Landscaping Requirements.

Recommendations

- 4.4-1 Landscaping should be used to clearly define the parking areas from the right-of-way and highlight parking lot entry and exit points.
- 4.4-2 Parking bays should be separated by a ten (10) foot landscaped strip, vertically faced with granite curbing. (figure 4.10)
- 4.4-3 One planting is recommended per parking space along one length of the island.
- 4.4-4 Landscaping strips are required between areas where pedestrian walkways and parking spaces abut.
- 4.4-5 In areas where parking lots have frontage on North Main St, a berm, a vegetative hedge, or hard buffer or any combination of these should be provided.



Figure 4.10

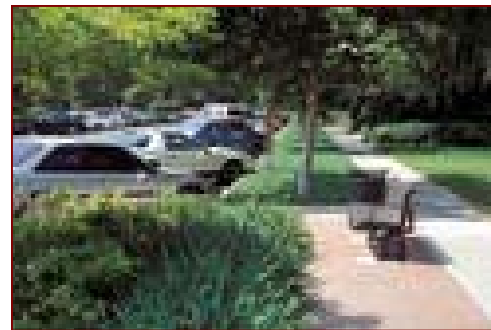


Figure 4.11

Landscaping and Screening

Hedges and Buffering

Intent: Buffering should be used to define the edges of various land uses and provide a transition between different uses. Essentially, buffering is an important tool that increases pedestrian safety by erecting both as a visual and physical barrier between the public right of way and private property. Vegetative covering along the perimeter of a building or a parking lot that has large imperious surfaces is encouraged in order to soften the building form and mask less attractive elements.

Zoning Requirements:

- 4.5-a Any portion of the front lot line that is not occupied by a building shall have a fence or brick wall along the front lot line. The fence shall be 42 inches high. All chain link, barbed wire or razor wire fences are prohibited.
- 4.5-b Where a vehicular use areas in any zone adjoins a lot in an R Zone, the area shall be screened by a solid wall, a uniformly painted tightboard fence, or a hedge of compact evergreens or other suitable plantings.

Recommendations

- 4.5-1 Suggested types for the fences and walls: hedges, shrubs, trees, various plantings, wooden fences, walls, stone, red brick and granite.
- 4.5-2 The height, size or extent of the buffering should be determined in a manner that effectively buffers outdoor containers, large paved surfaces and parking lots from public view.
- 4.5-3 The recommended height of hedges is 30". Recommended spacing of plantings in the hedge rows is dependent on the species of plant.
- 4.5-4 Vegetative buffering, fencing, other screening or a combination of these is strongly recommended along the perimeter of a commercial, industrial property or parking lot when it adjoins a residential property. (figures 4.12, 4.13)

Figures 12 and 13: Before and after photo enhancement of large parking lot with addition of wall for screening.



Figure 4.12



Figure 4.13

Landscaping and Screening

Screening and Fencing

Intent: Fencing, walls and screening act as a buffer between the public and private areas while improving comfort and safety for pedestrians. The intent is to keep fences and walls at a minimum functional height but still able to serve its intended purpose. Unified walls and fencing of the same building materials, color and style are encouraged but not to the extent that the corridor resembles a continuous barrier. Walls and fencing should be complemented with hedges, trees and plantings in order to break up large walled surfaces.

Zoning Requirements:

4.6.a (See zoning requirement for Hedges and Buffering)

Recommendations

- 4.6-1 Suggested types: hedges, shrubs, trees, various plantings, wooden fences, walls, stone, red brick and granite.
- 4.6-2 The appearance of fencing and screening shall be similar to the that of adjacent structures in terms of material, color, shape and size.
- 4.6-3 Fences, walls, screening or a combination of these is encouraged along the frontage of commercial and industrial uses.
- 4.6.3a Masonry walls maximum height (8 feet) and minimum (3 feet). Decorative metal fencing minimum (3 feet).
- 4.6-4 Any commercial business that has large parking lots, unimproved lots or a grade change that adjoins the public right of way should have some type of fencing, wall or vegetative screening.
- 4.6-5 To break the visual monotony of a street wall, one shrub or vine should be planted along the wall every 10 feet.
- 4.6-6 All commercial, industrial and multifamily residential uses should provide enclosed screening for trash, utility and cable infrastructure, and satellite receivers.

Figures 4-14 and 4-15 North Main Street auto business currently (4.14) and with photo-enhanced addition of brick wall for screening (4.15).



Figure 4.14



Figure 4.15

Signage

Intent: North Main Street is a significant entry corridor from Pawtucket toward Downtown Providence and as such signage is high visibility and can make a lasting impression. Signs should enhance the appeal of the corridor. They should be consistent with the adjacent or corresponding building form and effectively promote the local commerce. Their purpose is to inform and add interest, never to detract or cause a distraction.

Zoning Requirements:

- 5.1-a The maximum total area of all signs on any structure shall not exceed one and a half (1.5) square feet per one (1) foot of building frontage. Window signs shall not be included in this calculation.
- 5.1-b All signs may be illuminated externally or internally, except that internally illuminated signs are not permitted in HD overlay zones.
- 5.1-c Freestanding and roof signs are not permitted.
- 5.1-d Maximum area of window signs is two (2) square feet.
- 5.1-e Maximum limits for canopy signs are:
 - 1.5/1' of building frontage
 - 6 feet of projection over public ROW
- 5.1-f Maximum limits for projected signs are:
 - 20 square feet of area
 - 18 feet in height
 - 0 feet in setback
 - 6 feet of projection over public ROW
- 5.1-g Maximum limits for wall signs are:
 - 1.5/1' of building frontage
 - 1.25 feet of projection over public ROW

Recommendation

- 5.1. **Style.** Signs should reflect architectural elements of the building they accompany. Each building has its own style and character which should be represented in the sign's design with regard to color, size, bordering, etc.



Figures 5.1 and 5.2. Encouraged: Sign coordinated with the style and colors of the building. This is also an effective directory sign.



Styles

5.4 Wall Signs, General

Recommendations

- 5.4-1 Ground floor tenants should place signs below second floor windows without obscuring cornices or other architectural details.
- 5.4-2 Window signs retaining glass transparency are recommended for upper floor businesses.
- 5.4-3 Channel lettering is preferred to cabinet signs on the building façade unless the sign is specifically designed to compliment the building.
- 5.4-4 A wall sign may be on both street frontages on a building on a corner lot, through the secondary street sign should be smaller than the sign for the North Main Street frontage.
- 5.4-5 Signs on a row of storefronts on the same building should all be of a similar size, material and proportion.

- 5.4a. **Multiple-business buildings.** Wall signs should be compatible with each other in terms of type, size, color, illumination or font so that a unified appearance is achieved. Sign placement along the façade should be consistent; evenly spaced over business entrances where possible.



Figure 5.3 *Attractive channel lettering.*



Figure 5.4 *Simple but attractive channel lettering wall sign. Wall signs with similar type of lettering positioned over business entrances on commercial strip.*

Styles

5.5 Projecting Signs. (Any sign perpendicular to and projecting outward from the wall of a building more than 15 inches).

Recommendations

- 5.5-1 A sign bracket should extend the full length of the sign and be rigidly supported.
- 5.5-2 Brackets or mounting hardware should be decorative and should compliment aspects of the building.
- 5.5-3 The bottom of the sign should be a minimum of 8 feet above the sidewalk and should be small in scale, six (6) square feet or less. Projecting signs should be attached to the building below the second floor windowsill and should not extend above the roof of the building or be attached to the roof.



Figure 5.5. Encouraged : Colorful logo projecting sign.

5.6 Window Signs.

Recommendations

- 5.6-1 Window signs should retain glass transparency.
- 5.6-2 Paper signs and temporary signs attached directly on to the window (Figure 5.4) are strongly discouraged.
- 5.6-3 Any signs attached to a window advertising special sales should be removed after no more than 15 days and should cover no more than 25% of the window to which it is attached.
- 5.6-4 Signs may be placed inside of an establishment within three feet of the window and be illuminated, provided they are professionally designed and constructed.



Figure 5.6. Encouraged: Transparent window signs

Styles

5.7 Awning or Canopy Signs.

Recommendations

- 5.7-1 An awning or canopy sign may be combined with a wall sign, menu box or a window sign.
- 5.7-2 The lowest extent of an awning sign should be no less than 8 feet above the sidewalk.
- 5.7-3 It is recommended that awning should be a solid color, with no more than two additional colors of lettering as part of signage. Black and white count as colors.

5.8 Temporary Signs.

Recommendations

- 5.8-1 Temporary signs are strongly discouraged with the exception of real estate signs on properties for sale or rent, grand opening banners, and construction signs advertising the various construction trade groups at a site.
- 5.8-2 Construction signs should be removed within 60 days of the completion of construction
- 5.8-3 Grand opening banners should stay on the property for no more than 30 days.
- 5.8-4 Real estate signs should be removed within 30 days of the real estate closing or lease transaction.



Figure 5.7 *Awning with sign, complimenting colors of building design of building*

Lighting

5.9 Lighting.

Recommendations:

- 5.9-1 External illumination or illuminated channel letters are preferred.
- 5.9-2 External lighting should be shielded and aimed toward the sign without projecting past it.
- 5.9-3 Signs should remain illuminated after business hours.
- 5.9-4 Internally lit channel letters are effective and attractive for wall signs.
- 5.9-5 Internally lit plastic cabinet or box signs are discouraged. If cabinet signs are used, they should have a non-illuminated or opaque background, allowing light to show through the lettering and logo.
- 5.9-6 Signs featuring moving or flashing lights are prohibited.



Figure 5.9 Encouraged: Internally lit channel letter sign



Figure 5.8 Encouraged: Externally lit sign remains lit after business hours.



Figure 5.10 Acceptable: Internally lit cabinet sign with opaque background

Intent: The purpose of establishing a streetscape for North Main Street is to offer visitors to the area an aesthetically pleasing experience. A sense of identity can be achieved along the length of the commercial corridor through the use of streetscape amenities such as benches, trash receptacles, bus stops in coordination with RIPTA, planters, kiosks, and bicycle racks. All of these components should be contained within the public right-of-way, be integrated as part of the site design, and promote the existing architectural style of the area.

Recommendations:

- 6.1-1 Benches should be provided within reasonable walking distance of one another.
- 6.1-2 The style of the benches should coordinate with the architectural style of both the buildings and the surrounding areas. (6.1)
- 6.1-3 Planters should be affixed to the side of the building or located in such a way as to not obstruct pedestrian traffic. (figure 6.2, 6.3)
- 6.1-4 Planters should reflect the materials and style of adjacent buildings and should be kept in good condition.
- 6.1-5 Placement of kiosks should be considerate of the public right-of-way and not hinder access.



Figure 6.1



Figure 6.2



Figure 6.3

continued

- 6.1-6 Kiosks should be landscaped on all sides except the point of entry and be kept in good condition, as well be kept free of flyers and miscellaneous signage.
- 6.1-7 One trash receptacle per block is recommended along sidewalks.
- 6.1-8 Trash receptacles should be made of durable material, bolted to the pavement, and provide a covering to guard against rain and animals. (figure 6.6, 6.7)
- 5.1-9 Visible clutter should be reduced along the street side by combining amenities as often as possible (figure 6.4). An example would be benches combined with planters or trash receptacles.
- 6.1-10 Streetscape amenities should be consistent along the corridor.
- 6.1-11 Bike racks and bus stops should be located along the corridor at the plazas. (See Public Spaces)
- 6.1-12 Bus shelters need to provide protection from the elements such as a roof or enclosure. (figure 6.5)



Figure 6.4 *Several uses on one amenity*



Figure 6.5



Figures 6.6 and 6.7



Figure 6.8

Plazas

Intent:

Plazas play an important role in our vision for North Main Street. They should serve as a linkage between the different sections of the street, and represent nodes for pedestrians. They also should be a gathering place and an entry point into the street from the neighborhoods. The development of plazas creates an opportunity for public/private partnerships.

A process to design the plazas should involve the neighborhoods of Hope and Mount Hope. A design contest could be held offering the neighborhoods the opportunity to create a theme for each plaza. This would create a connection between the neighborhoods and the plazas which may not exist otherwise.

Recommendations:

- 7.1-1 Location: The plazas should be placed at the following intersections: North Main Street and Doyle, Branch, Rochambeau, Cemetery, Smithfield, and at the entrance from Pawtucket.
- 7.1-2 Design: The plazas should form a circle around the intersection where possible with catchment areas defined specifically for each plaza according to the available space.

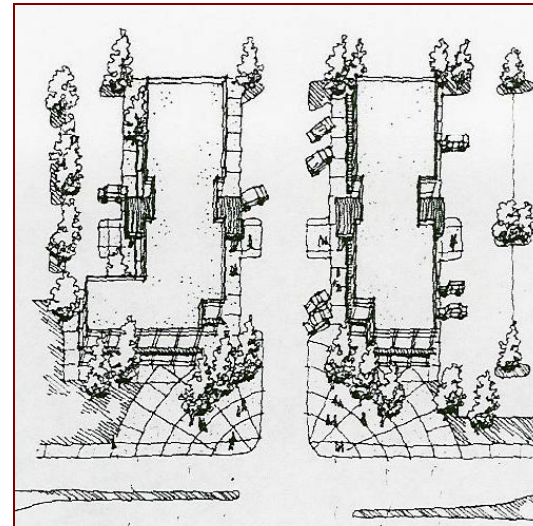


Figure 7.1 *The plaza can be on one side of the street*

Plazas

Recommendations:

Connection and Access:

- 7.1-3 Plazas should be connected to the transit system, with a bus shelter close to the plaza on both sides of the street.
- 7.1-2 It is strongly recommended that access to plazas and businesses are ADA compliant making it easier for strollers as well.
- 7.1-3 The corner plazas should provide a connection between North Main Street and the side streets and invite pedestrians to do business on the street.
- 7.1-4 Way finding signs which provide an orientation map as well as directions to transit stops, taxi stands, nearby neighborhoods streets are encouraged.
- 7.1-5 A kiosk which can provide information board is recommended. This supplies the neighborhood with a place to announce community events.

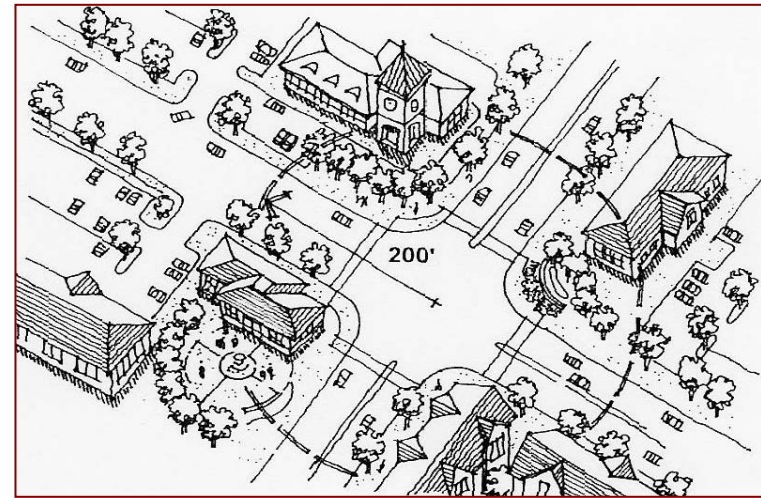


Figure 7.2 *The plaza can encompass both sides of the intersection*

Plazas

Recommendations:

- 7.1-6 The road surface at these intersections should be paved with a special material such as cobblestone, making the plazas visible and recognizable to drivers.
- 7.1-7 Impervious surfaces should be minimized and green areas maximized.

Climate:

- 7.1-8 In consideration of the New England climate, plazas should be habitable during the summer months, and inviting during the winter months.
- 7.1-9 Provision of filtered shade by means of a canopy of deciduous trees is encouraged.
- 7.1-10 When possible, the use of buildings and landscaping to protect the plaza from cold winds is strongly encouraged.
- 7.1-11 Whenever possible, provide a protection against rain and snow through use of awnings or arcades.



Figure 7.3

Plazas

Recommendations:

- 7.1-12 Promote diversity in retail offerings near plazas to encourage day-time use by workers, shoppers and tourists. This could include food shops, coffee shops, bakeries, and movie cinemas.
- 7.1-13 Design plazas to accommodate vendors.
- 7.1-14 Quality seating areas are strongly encouraged to promote the use of the plaza with:
 - At least 2 benches on each side of the intersections, one linear foot of seating per each perimeter foot of plaza
 - At least 50% of the seating of a secondary nature, in the form of steps, planter seats, or retaining wall.
 - Provision for different types of seating, including circular seating to encourage interaction.
 - Seating availability in both sun and shade.
- 7.1-15 Provision of street furniture such as trash cans, public telephones, and bike racks is strongly encouraged.
- 7.1-16 To ensure the visibility of these plazas, a banner, consistent in design with other banners on the street, should be placed on one side of the intersection.
- 7.1-17 The lighting of corners and out of the ways locations is strongly recommended.
- 7.1-18 Make sure the corners and out of the ways locations are lighted
- 7.1-19 Inclusion of public art in the plaza design is encouraged.



Figure 7.4

Plazas

Recommendations:

- 7.1-20 The side street coming down from the hill should be one-way to increase the safety for pedestrians.
- 7.1-21 A raised crosswalk is recommended in addition to a pedestrian cycle on the signal light.
- 7.1-22 Lighting should be designed in a way that provides safety to pedestrians.
- 7.1-23 Auto related, drive through, and formula businesses should be discouraged from locating in close proximity to the plaza. For existing businesses, screening should be utilized.
- 7.1-24 Business associations or neighborhood associations are encouraged to maintain plaza areas.



Figure 7.5

Corner Lots

Intent: The corners are of significant importance for pedestrian traffic. Plaza and corner lots are places of convergence, where people wait to cross and are more likely to converse with others. On North Main Street, those corner lots are in bad shape, poorly maintained and not laid out to attract pedestrians and enhance the aesthetic of the street. This section is about the corners that are not part of the envisioned plazas.

Recommendations:

- 7.2-1 Buildings located on corner lots should have zero feet of setback from both North Main Street and the side street.
- 7.2-2 Sidewalk landscaping at the corners should consist of mature trees and green strips as demonstrated in Figure 7.6
- 7.2-3 The curb radius should be small for traffic calming.
- 7.2-4 Seating should be provided whenever possible.



Figure 7.6

Open Spaces

Intent: Large sites are encouraged to incorporate pedestrian walkways and open spaces to create small breaks in the street wall to encourage movements through the site towards the adjacent neighborhoods.

Recommendations:

- 7.3-1 The largest open space along the street is the cemetery. This asset should be taken advantage of in two ways:
- as an aesthetically pleasing open view for the properties located on the opposite side of the street. This could encourage residential development and increase density.
 - as a passive recreation area for neighborhood residents. This would require other entrances to the cemetery, for instance across from the intersection with Rochambaud Ave. This particular access would benefit from the plaza and the enhanced sidewalk envisioned at this here.

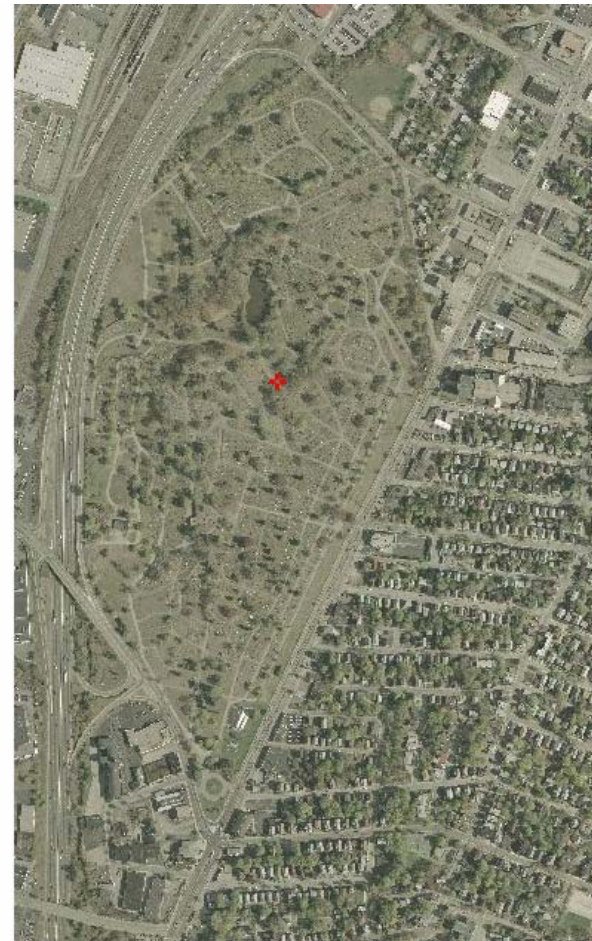


Figure 7.7

V- References

1. Site Planning and Orientation

- Figure 1.1 Westerly, RI, October 2004
Figure 1.2 “Design Guidelines for Commercial Façade Improvements”, City of Philadelphia, 2003.
<http://www.philaplanning.org/plans/designguide.pdf>
Figure 1.3 Livermore, CA: “Design Standard and Guidelines: Commercial”
http://www.ci.livermore.ca.us/City_design_guidelines/chapter_5.pdf
Figure 1.4 Philadelphia: “Design Guidelines for Commercial Façade Improvements”, 2003.
<http://www.philaplanning.org/plans/designguide.pdf>

2. Building Form and Design

- Figure 2.1 Westerly, RI, October 2004
Figure 2.2 Boulder, CO: “Downtown Urban Design Guidelines”, 2002.
Figure 2.3 Livermore, CA: “Design Standard and Guidelines: Commercial”
http://www.ci.livermore.ca.us/City_design_guidelines/chapter_5.pdf
Figure 2.4 Boulder, CO: “Downtown Urban Design Guidelines”, 2002
Figure 2.5 Boulder, CO: “Downtown Urban Design Guidelines”, 2002.
Figure 2.6 Philadelphia: “Design Guidelines for Commercial Façade Improvements,”2003.
<http://www.philaplanning.org/plans/designguide.pdf>
Figure 2.7 Charlottesville, VA: “Charlottesville Entrance Corridor Design Guidelines”, 2003.
Figure 2.8 Boulder, CO: “Downtown Urban Design Guidelines” 2002.
Figure 2.9 Ibid.
Figure 2.10 North Main Street, Providence, RI, picture by Wayne Barnes, October 2006.
Figure 2.11 Ibid.
Figure 2.12 Ibid.
Figure 2.13 Ibid.

Figure 2.14	Boulder, CO: “Downtown Urban Design Guidelines”, 2002.
Figure 2.15	Philadelphia: “Design Guidelines for Commercial Façade Improvements”, 2003. http://www.philaplaning.org/plans/designguide.pdf
Figure 2.16	Boulder, CO: “Downtown Urban Design Guidelines”, 2002.
Figure 2.17	Boulder, CO: “Downtown Urban Design Guidelines”, 2002.
Figure 2.15	Philadelphia: “Design Guidelines for Commercial Façade Improvements”, 2003.

3. Parking

Figure 3.1	Livermore, CA: “Design standard and Guidelines: Commercial”, at http://www.ci.livermore.ca.us/city_design_guidelines/chapter_5.pdf
Figure 3.2	Kalispell, MT: “Montana Architectural Design Standards” http://www.kalispell.com/downloads/arc_standards.pdf
Figure 3.3	idem
Figure 3.4	Amanda Harris, <u>Designing With Climate: Using Parking Lots to Mitigate Urban Climate</u> http://scholar.lib.vt.edu/theses/available/etd-11212004-190947/unrestricted/AHarrisThesis.pdf
Figure 3.5	Livermore, CA: “Design standard and Guidelines: Commercial”, at http://www.ci.livermore.ca.us/city_design_guidelines/chapter_5.pdf
Figure 3.6	Kalispell, MT: “Montana Architectural Design Standards” http://www.kalispell.com/downloads/arc_standards.pdf
Figure 3.7	Charlottesville, VA, “Charlottesville Entrance Corridor Design Guidelines”, 2003.
Figure 3.8	Village of North Randall: Master Plan http://cpc.cuyahogacounty.us/docs/masterplans/northrandall.pdf
Figure 3.9	Harris A.M. “Designing with climate: using parking lots to mitigate urban climate” at http://scholar.lib.vt.edu/theses/available/etd-11212004-190947/unrestricted/AHarrisThesis.pdf
Figure 3.10	At http://www.afcee.brooks.af.mil/dc/dcd/land/ldg/s14ParkingAreas/c02DesignConsiderations.html
Figure 3.11	Kalispell, MT: “Montana Architectural Design Standards” http://www.kalispell.com/downloads/arc_standards.pdf
Figure 3.12	Kalispell, MT: “Montana Architectural Design Standards” http://www.kalispell.com/downloads/arc_standards.pdf
Figure 3.13	idem
Figure 3.14	At http://www.afcee.brooks.af.mil/dc/dcd/land/ldg/s14ParkingAreas/c02DesignConsiderations.html

4. Landscaping

Figure 4.1	Westerly, RI, picture by Susan Clark, October 2005
Figure 4.2	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.3	Livermore, CA: “Design Standards and Guidelines: Commercial”

	http://www.ci.livermore.ca.us/City_design_guidelines/chapter_5.pdf
Figure 4.4	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.5	Providence, RI, North Main Street, picture by Wayne Barnes and Joseph Maia, October 2005
Figure 4.6	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.7	Providence, RI, North Main Street, picture by Wayne Barnes and Joseph Maia, October 2005
Figure 4.8	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.9	“Design Standards and Guidelines: Commercial” Livermore, CA; http://www.ci.livermore.ca.us/City_design_guidelines/chapter_5.pdf
Figure 4.10	“Design Standards and Guidelines: Commercial” Livermore, CA; http://www.ci.livermore.ca.us/City_design_guidelines/chapter_5.pdf
Figure 4.11	Livermore, CA: “Design Standards and Guidelines: Commercial”
Figure 4.12	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.13	Providence, RI North Main Street, picture by Wayne Barnes and Joseph Maia, October 2005
Figure 4.14	Providence, RI, North Main Street, picture by Wayne Barnes, October 2005
Figure 4.15	Providence, RI, North Main Street, picture by Wayne Barnes and Joseph Maia, October 2005

5. Signage

Figure 5.1	Providence RI, North Main St., picture by Wayne Barnes, September 2005
Figure 5.2	Providence RI, North Main St., picture by Wayne Barnes, September 2005
Figure 5.3	Providence RI, North Main St., picture by Wayne Barnes, November 2005
Figure 5.4	East Providence RI, Taunton Ave. , picture by Wayne Barnes, November 2005
Figure 5.5	Providence RI, Thayer St., picture by Wayne Barnes, November 2005
Figure 5.6	East Providence, RI, Waterman Ave. , picture by Wayne Barnes, November 2005
Figure 5.7	Providence RI, North Main St., picture by Wayne Barnes, November 2005.
Figure 5.8	East Providence, RI, Massasoit Ave., picture by Wayne Barnes, November 2005
Figure 5.9	“Albemarle County Architectural Review Board Sign Guidelines”, Albemarle County, VA http://www.albemarle.org/deptforms.asp?section_id=&department=cdarb
Figure 5.10	Willett Ave., Riverside, RI, picture by Wayne Barnes, November 2005

6. Streetscape

Figure 6.1	TimberForm Site Furniture at http://www.timberform.com/index.cfm?content.display&pageID=78
Figure 6.2	idem
Figure 6.3	Charlottesville, VA, “Charlottesville Entrance Corridor Design Guidelines”, 2003.
Figure 6.4	idem
Figure 6.5	Charlottesville, VA, “Charlottesville Entrance Corridor Design Guidelines”, 2003.
Figure 6.6	Art.Lebedev Studio, Trashman’s Dream: http://www.artlebedev.com/mandership/88/
Figure 6.7	TimberForm Site Furniture at http://www.timberform.com/index.cfm?content.display&pageID=78
Figure 6.8	idem

7. Public spaces

Figure 7.1	Overland park, Kansas design guidelines
Figure 7.2	Overland Park, Kansas, design guidelines, p-20
Figure 7.3	Livermore, CA, draft design Guidelines, p-220
Figure 7.4	Charlottesville, VA, “Charlottesville Entrance Corridor Design Guidelines”, 2003.
Figure 7.5	Providence, RI, Atwells Av , picture by Carol Tolchinsky, Nov 23, 2005
Figure 7.6	Livermore, CA, draft design Guidelines, p-220
Figure 7.7	Providence, RI, aerial photography

8. Literature

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Town of Sharon, Massachusetts: *Post Office Square Design Guidelines*

Town of Windham, Connecticut: *Design Guidelines for Windham Business (B-1) District*, revised May 12, 2005

VI- Appendices

- A. Glossary
- B. Notes from the Workshop with providence Planning Department, Monday November 7th 2005
- C. Notes from the Summit neighborhood association meeting on October 20th 2005 (at Javaspeed)
- D. Final presentation to the Providence Planning Department

A- Glossary

- A
- *Amenity* – Aesthetic or other features of a development that increase its marketability or usability to the public.
- *Articulated Rooflines* – *The roofline effect created by adjacent buildings of varying heights*
- B
- *Bracket* – An inverted L-shaped architectural support, typically supporting an overhang, beam or other horizontal surface.
- *Buffer* – Land maintained in either a natural or landscaped state, used to screen and/or mitigate the impacts of development on surrounding areas, properties or right-of-ways.
- *Bulkhead* – The areas that support the display windows; can be of wood or metal.
- C
- *Canopy Tree* – Any species of deciduous tree, normally growing to a mature height of forty feet or more, with a broad foliage crown.
- *Column* – An exposed, usually decorative, vertical architectural support.
- *Cornice* – A molded or projecting horizontal feature that crowns a façade.
- D
- *Display Window* – The main areas of clear glass on a storefront behind which goods are arranged.
- E
- *Exterior Insulated Finishing Systems (EIFS)* – Multi-layered exterior wall systems, typically consisting of insulation board, a durable, water-resistant base coat and a durable finish coat. (Defined at <http://www.eima.com>)
- F
- *Façade* – Any non-structural surface on an exterior wall distinguished by decorative details.
- *Finial* – A decorative terminal form at the top of a feature.
- *Freestanding Sign* – A sign supported by a post or legs, or placed at a distance from a building.

A- Glossary

▪ L

- *Lintel* – Structural member above a storefront that supports the parapet or upper wall.
- *Lot Frontage* – That portion of a lot abutting a street.
- *Lot Line* – A line of record, bounding a lot, which divides one lot from another lot or from a public or private street or any other public or private space.
- *Front* – The lot line separating a property from the primary (building address) street right-of-way.
- *Rear* – The lot line opposite and most distant from the front lot line.
- *Side* – Any lot line other than a front or rear lot line.

▪ P

- *Parapet* – A low wall used as protection in any location where there is a drop; such as at the edge of a roof, balcony or terrace.

▪ R

- *Roof styles -*
- *Gable* – The typical double pitched roof.
- *Hipped* – A roof with four separate pitches, terminating either at a point or at a common ridgeline .
- *Mansard* – A roof that pitches steeply upward from the top of a wall for a brief span, usually one story or less, then either terminates in a flat roof or continues at a more modest pitch to the ridgeline. Sometimes dormer windows are placed within the mansard roof section.
- *Flat* – A roof with little or no pitch, usually covered either with a waterproof membrane or asphalt and pea-stone.
- *Barn-style* – A roof with a steep initial pitch that breaks at some point into a more modest pitch that meets the opposing side at a common ridgeline.
- *Rosette* – A typically circular motif, concealing the end of a bar that helps secure the lintel to the structure behind.

▪ S

- *Sash* – A frame around the glass of a window..

A- Glossary

- *Scale* – The size of a building or environmental feature, and the relationship of that size to the surrounding built or natural environment.
- *Setback* – A minimum distance between the street line and a portion of a façade parallel to the street line as designated by the zoning regulation.
- *Sill* – Horizontal member immediately below a window, or a beam that runs around the outside perimeter of a building that supports the floor joists and exterior walls.
- *Storefront* – The front exterior wall of commercial space, typically with large areas of glass.
- *Streetscape* – The visual and functional character of an entire street including: buildings, paving material, plantings and street amenities such as lamps and benches. Also, the environment created for human activity and interaction.
- *Streetwall* – A wall set on a frontage line, in the absence of a façade.
- T
- *Transom* – Upper windows in a storefront; can be operable or fixed, clear or patterned.
- *Tree Caliper* – The trunk diameter of a tree.
- U
- *Understory Tree* – Any species of tree that normally grows to a mature height of fifteen to thirty-five feet.
- *Upper Floor* – The usually non-retail space above the retail ground floor; multistory Main Street buildings are typically between two (2) and four (4) floors.
- W
- *Window Hood* – A projecting structure above a window, on Main Street typically of wood or cast iron.

B- Workshop, Monday November 7th 2005

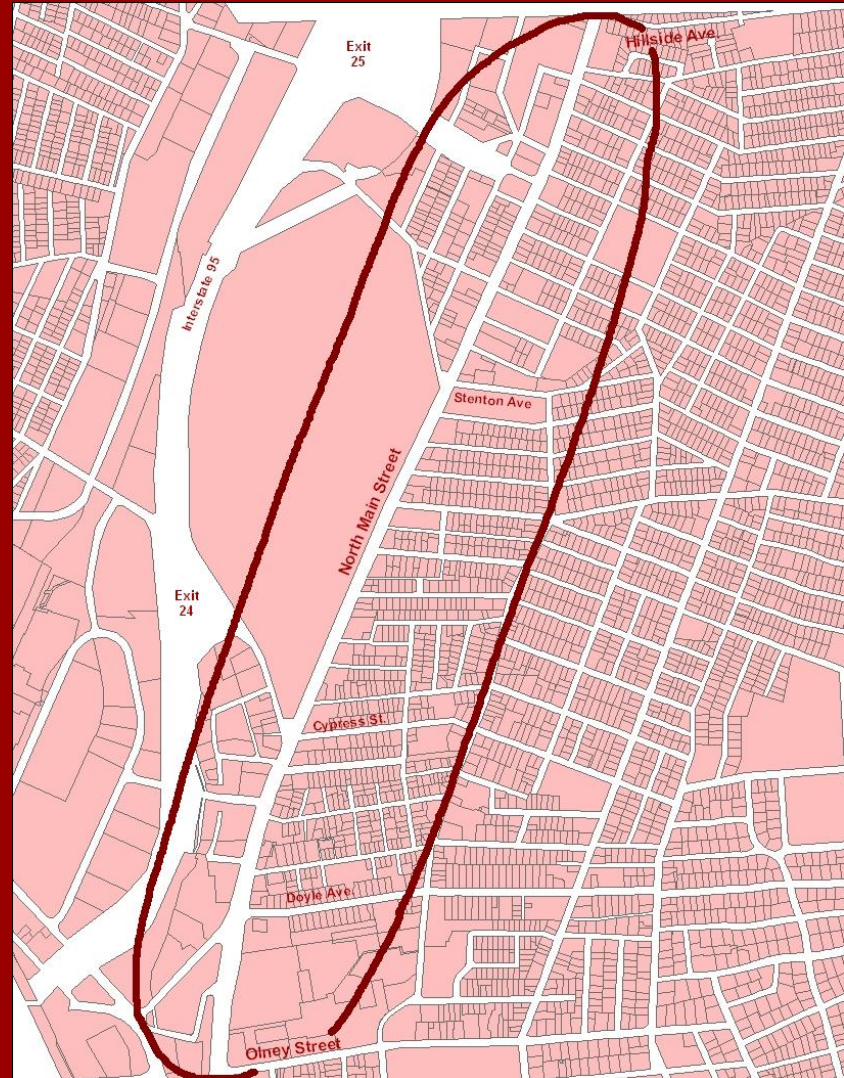
- Footbridge on the Doyle intersection: good idea
 - To increase pedestrian safety: add a pedestrian cycle to the street lights at the Doyle intersection
 - For landscaping improvement; the Summit Neighborhood should compete for the grants offered through the street market project, and then affect this grants to individual business owners
 - Auto businesses:
 - the idea of having the area along Branch Av made compliant to environmental regulations, in order to propose a relocation to the auto businesses there is not realistic because there is nothing for sale along Branch av
 - But the idea of CA zoning along Branch Av and along Smithfield Av is good: they will have drivers coming
 - Have them checked for their compliance with environmental regulations by DEM
 - Bike route: good idea
 - Street car: why not> Density issues
 - One-way street for the street coming down from the hill:
 - Yes it is a good idea, to increase safety for pedestrians
 - But in certain cases people on the side street will have to drive up to the street and find another side street to access Main street, and a one side street sometimes make people go faster.
- Plaza: Good idea
 - With crosswalk and local businesses
 - Streets could be paved differently; like cobblestone
 - Add some community boards for people to announce events, yard sales...
 - - We should see
 - where we can accommodate Height
 - what kind of retail by node
 - good idea to try to create a night life with restaurant and movie theater
 - think about using the cemetery as a recreation space for walking
- Questions for DOT:
 - why the middle strip is not landscaped after Frost street?
- Questions for Sam Shamoon in PPD:
 - about the abandoned gas station: is there a building inspection or permit for this place? (call him with the address of the building)
 - when an already existing auto business stops and sell to a new owner: is the new owner entitled to go on the auto business when it is not compliant to the zoning?

C- Summit Neighborhood Association, October 20th 2005

- What is wanted:
 - Beautiful entry to Summit—Redesign of Smith St.
 - Feel safe, protected
 - Smithfield Ave. lit with reflectors
 - No abandoned or vacant properties
 - Less obtrusive signs
 - Demographic and traffic studies
 - Safe pedestrian crossings
 - Family friendly stores ex. Gregg's—moderately upscale
 - Similar to Sandwich Hut, Yoga studio, Javaspeed
 - Businesses for nightlife
 - Bookstore
 - An identity for North Main (North Main Market, brand for N. Main)
 - Full range of shopping from 3rd to City line
 - Parking garage
 - Design for a Main St feel
 - Zero setbacks, 3-4 stories, retain on bottom floor
 - Peds and motorists can co-exist on busy streets ex. 5th Ave, NYC
 - BUT—located next to I-95
- First stage:
 - Be able to park and walk to a few stores within one block
 - Be able to cross the street, have a walk cycle on the lights
- Difficulties
 - Hard to make it totally walkable, residences are an uphill climb
 - Intersection with Smithfield is very bad
- Discussion about the height of the buildings especially on the east side.
 - View would be lost with 4-6 story bldgs.
 - Creation of a walkable area means a need for greater density
 - Street wall creates a sense of security
- Would like to recreate a street with a large anchor store like Trader Joe's
- Possibilities:
 - Look to college students to do market studies to determine what businesses might come in
 - Begin a community street watch program

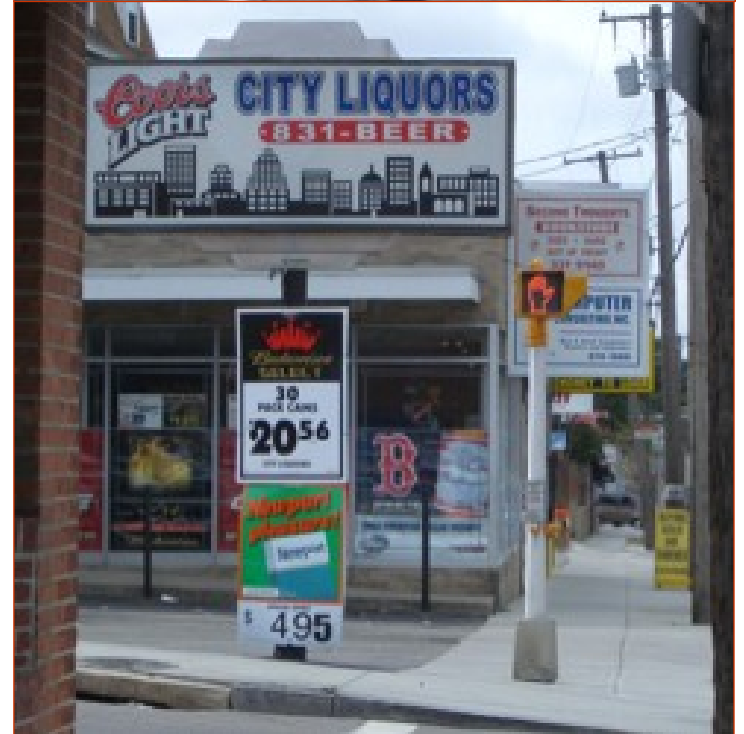
D- Final presentation NORTH MAIN STREET

Wayne Barnes
Susan Clark
Françoise Flourens
Melina Gesch
Joseph Maia



Overview

- Street opportunities and constraints
- Vision for the street
- Recommendation for the short-term
- Recommendation for the long-term



Significant constraints

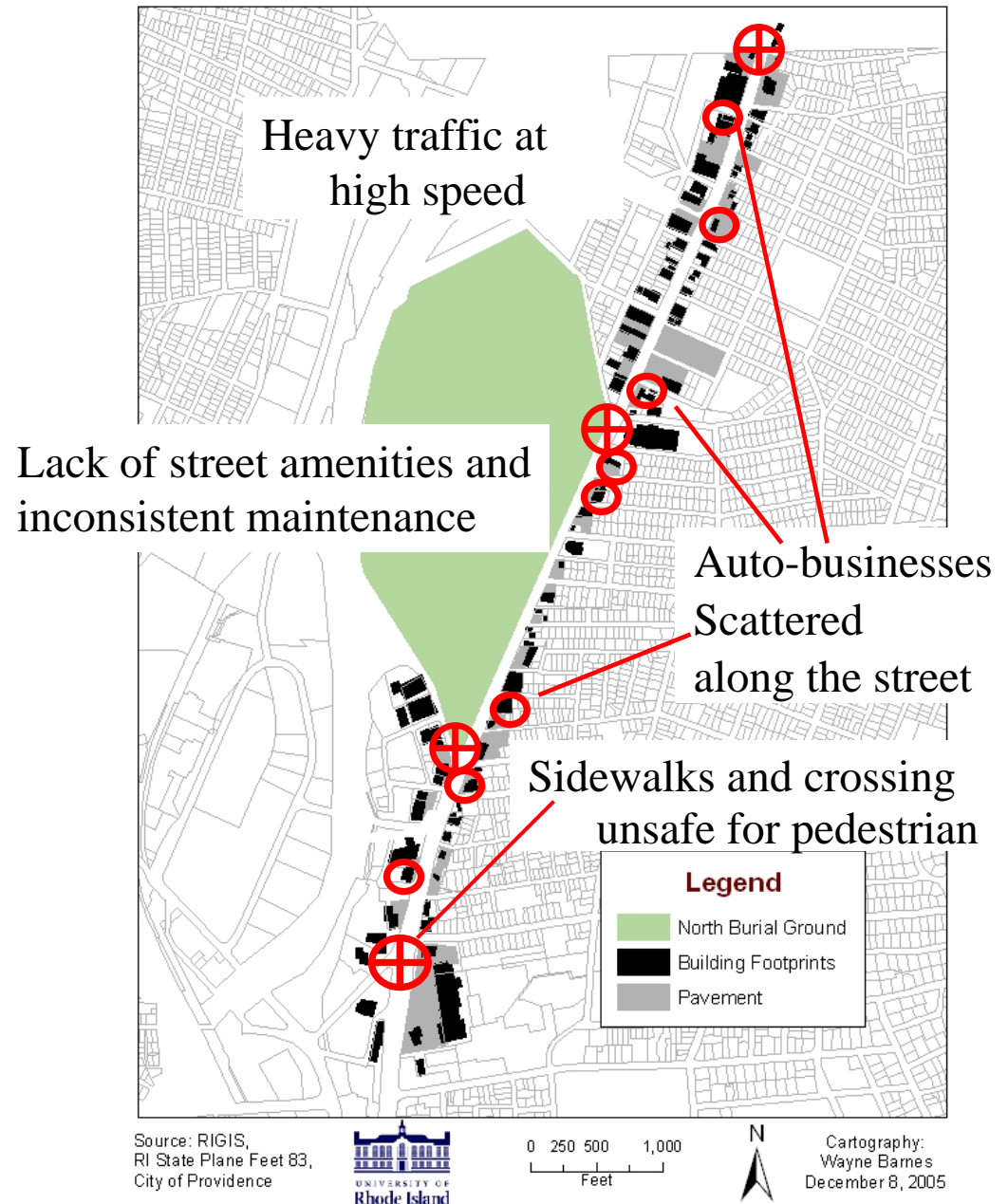
Poor street level attractiveness



Off-street pavement:
45% of the frontage

Lack of local businesses
to serve adjacent
neighborhoods

North Main Street Figure Ground



North Main Street Figure Ground

Some real opportunities

Historic/aesthetic buildings



Active neighborhood
Association: SNA



Large
Vacant lots

Steepness to
accommodate
building height

Cemetery: Open space
for view and recreation

Landscaping efforts

Economic Assets:
Miriam Hospital,
University Heights
Shopping Center

Transit system:
RIPTA # 99



Source: RIGIS,
RI State Plane Feet 83,
City of Providence



0 250 500 1,000
Feet



Cartography:
Wayne Barnes
December 8, 2005

Vision

Three sections articulated by plazas at

- ✧ Branch Ave.
- ✧ Rochambeau Ave.
- ✧ Cemetery St.
- ✧ Smithfield Ave.
- ✧ Hillside Ave.

North Main Street Study Area Sections and Section Goals

Section 1 Main street

Goal: develop a
“main street” business district

Section 2 Along the cemetery

Goal: ensure a safe
cohabitation
between pedestrians
and cars & increase
street-level appeal

Section 3 Southern Gateway

Goal: ensure pedestrian
safety and promote access to
the entire street

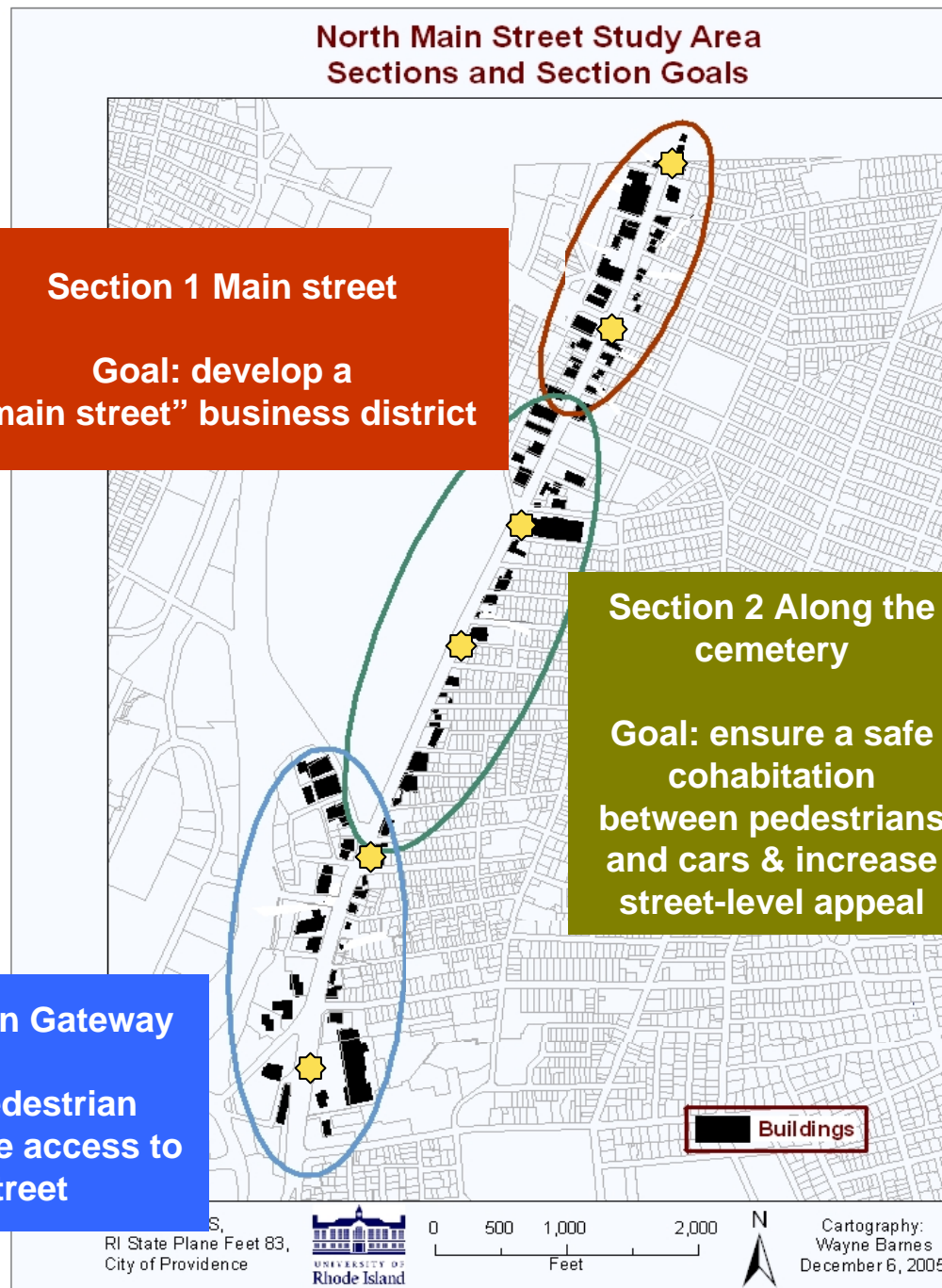
RI State Plane Feet 83,
City of Providence



0 500 1,000 2,000
Feet



Cartography:
Wayne Barnes
December 6, 2005



Vision

A safe and walkable street

- A street you can cross:
 - pedestrian bridge at Doyle
 - enhanced crosswalks
 - pedestrian-only cycle on traffic light
 - traffic calming
- Attractive, safe sidewalks
- Plazas to give a perspective to the pedestrians



Providence, RI, picture by F.Flourens on Dec 2005, with pedestrian bridge from Dunant-Steg, Stuttgart, Germany, 8 June 2000 by Nicolas Janberg

Vision

An attractive and vibrant streetscape

- Appealing street wall and windows
- Beautification of buildings in a consistent architecture style
- Landscaping to protect the pedestrian and improve the aesthetic of the street
- Valorization of the cemetery as an open space for the view and passive recreation
- Mixed use with small businesses and restaurants
- Plazas to host transit stops and create visual interest



Livermore, CA, draft design Guidelines, p- 220

Vision

A street connected to
its neighborhoods
and to downtown

- Street corners to connect with the side streets
- Gateways to promote the street businesses
- Plazas
 - designed with neighborhood involvement
 - favoring interactions
- A street car to link the neighborhoods to other activity center: Downtown and south of Providence (Cranston...)



Providence, RI, Atwells plaza, picture by C. Tolchinsky, Nov 23, 2005

On the short-term

Building form: windows

- ✓ Recommendation: Facades shall have lots of transparent glass.

Before



After



Providence, RI, picture by W.Barnes, Oct 2005

On the short-term

Building form: windows

- ✓ Recommendation: Windows should display merchandise and be lit up at night for safety



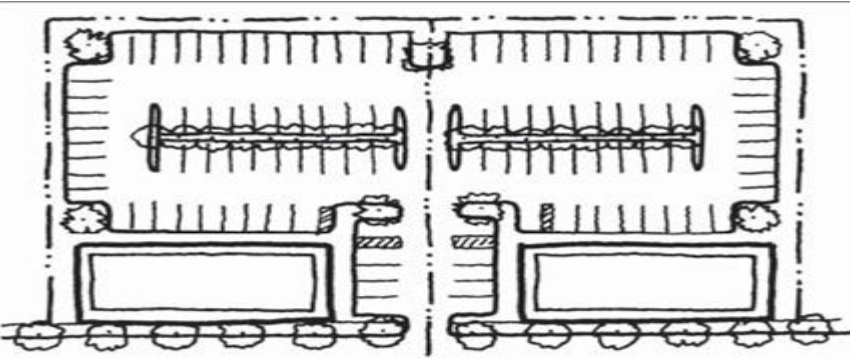
???,



On the short-term

Parking

- ✓ Recommendation: on street parking and shared parking should be encouraged whenever possible



Providence, RI, picture by W.Barnes, Oct 2005

On the short-term

Signage: Cluttering of signs

- ✓ Recommendation: Avoid cluttering of signs outside of the building and discourage paper and temporary signs attached to windows

Before



Providence, RI, picture by W.Barnes, Oct 2005

After



On the short-term

Signage: Matching with building

- ✓ Recommendation: Sign should reflect architectural elements of the building they accompany.



Providence, RI, picture by W.Barnes, Oct 2005

On the short-term

Landscaping on the raised divider island

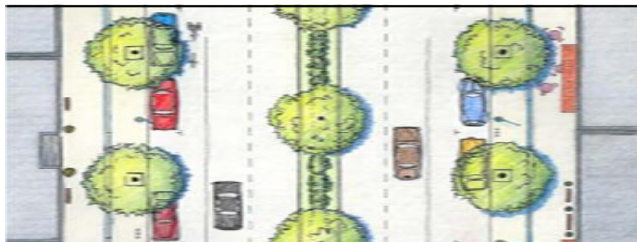
- ✓ Recommendation: Have a continuous unified canopy along the island

Before

After



Providence, RI, picture by W.Barnes, Oct 2005



On the short-term

Landscaping on sidewalks

- ✓ Recommendation: Have a continuous green strip with trees

Before



Providence, RI, picture by W.Barnes, Oct 2005

After



On the short-term

Landscaping: Auto Related Parking Lots

- ✓ Recommendation: Large expenses of impervious surface adjoining the public right-of-way should have some type of fence, wall or vegetative screening

Before



*Providence, RI, picture by
W.Barnes, Oct 2005*

After



On the short-term

Landscaping: screening parking lots

- ✓ Recommendation: Landscaping should be used to screen parking lot from the sidewalk and to treat runoff.

Before



Providence, RI, picture by W.Barnes, Oct 2005

After



On the short-term

Street amenities and lighting:

- ✓ Recommendation:
 - ✓ Street amenities should be clustered onto the plazas,
 - ✓ Lighting should be designed for pedestrians.



On the long-term

Building architecture

- ✓ Recommendation: Design facades with human scale in mind

Before



Providence, RI, picture by W.Barnes, Oct 2005

After



On the long-term

Building architecture

✓ Recommendation: Material, colors and textures should be consistent with the predominant red brick historic buildings of the street.

Before



Providence, RI, picture by W.Barnes, Oct 2005

After



On the long-term

Building improvement

Before



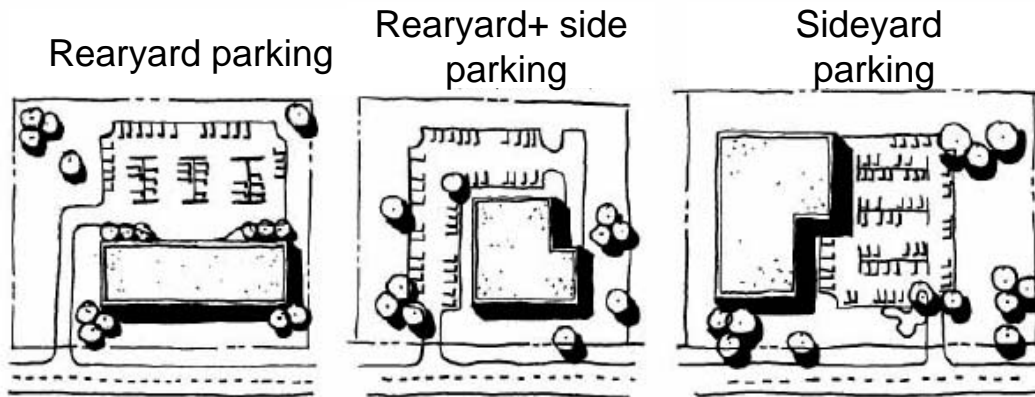
After



On the long-term

Parking

- ✓ Recommendation: Parking should be located on the rear, or on the side of the building, or underground. The favored solution would be to have a shared parking garage.



On the long-term

Public spaces

- ✓ Recommendation: Corner lots should be improved and designed to encourage human interactions and the connection with adjacent streets

Before



After



Conclusion

A multi-modal street
connected to its neighborhoods

